

PUBLIC RECORD OFFICE

THE PAGES IN THIS VOLUME ARE TOO
TIGHTLY BOUND FOR ALL WORDS TO BE
REPRODUCED IN ENTIRETY

Business Cards

29-1-1903

Draft

100 5 Jan, 1902

MR A. B. Hoyle

The Maas

Rhayader

Radnorshire

L. h.

Sir,

In reply to your
letter of 30th ultimo,
I am directed by the
Marquess of Lansdowne
to say that no report
has yet been received
from H. H. Munster at
Rething on the subject
of your dismissal by
the British Railway

Administration

A copy of your
letter will be sent
to Sir E Satow for his
information

21st January 1903.

Confidential.

Sir,

I am directed to forward herewith, for the
information of the Secretary of State for Foreign
Affairs, a Military Report on Imperial Railways in
China under the British Railway Administration.

I have the honour to be, Sir,

Your obedient Servant,

Meddman
Major General,

Military Secretary.

The Under Secretary of State,
Foreign Office.

9 Cl: bon
Mr A D Hoyle
46. June 1903
[Rg h: 30 x c: 2]
Mr A D Hoyle's Account
to Dept has yet
been received. Re this
will be forwarded to Dept

SECTION I - T'ANG-KU TO PEKING.

MAPS.

(I) General map of railway PLANS.

- (1) T'ANG-KU and HSIN-HO.
- (2) CHUNG-KUA-CHUNG.
- (3) TIENTSIN Station.
- (4) YANG-TS'UN.
- (5) LANG-FANG and German Post. (2 Plans)
- (6) KUANG-TE'UN.
- (7) FENG-T'AI Station and Post.
- (8) CH'EN-MEN and Post etc.
- (9) LU-KOU-CH'IAO extension.
- (10) TUNG-CHOU.
- (11) Specimens "HALF-DECK" "HALF-THROUGH" and "THROUGH" bridges.
- (12) Comparative diagrams of rates etc.
- (13) Plan of Tunnel through Hata Men Basin

(II) Map of T'ANG-KU to PEKING Section etc

- (1) CHUN-LIANG-CH'ENG.
- (2) Country round TIENTSIN Station.
- (3) PEI-TS'ANG.
- (4) LO-FA.
- (5) AN-T'ING.
- (6) Country round FENG-T'AI Station.
- (7) YONG-TING-MEN etc.
- (8) CH'EN-MEN Station.
- (9) SHUANG-CH'IAO.
- (10) Specimens "DECK" bridges.

SECTION II T'ANG-KU TO SHAN-HAI-KUAN.

MAPS.

(III) T'ANG-KU TO SHAN-HAI-KUAN section with the CHIN-FANG-TAO Extension. PLANS.

- (1) T'ANG-KU and HSIN-HO.
- (2) CHAN-TING.
- (3) LU-T'AI.
- (4) HUI-TO-CHUANG.
- (5) H'AI-T'ING.
- (6) Country round KU-YER.
- (7) LUI-CHUANG.
- (8) SHU-MEN.
- (9) CHANG-LI.
- (10) PEI-TA-HO.
- (11) Country round SHAN-HAI-KUAN.
- (12) PEI-T'ANG.
- (13) HAN-KU.
- (14) T'ANG-FANG.
- (15) TONG-SHAN.
- (16) HA-LI.
- (17) KU-YER Station.
- (18) LAN-CHOU.
- (19) AN-SHAN.
- (20) LIU-SHOU-YING.
- (21) T'ANG-HO etc. (2 plans)
- (22) SHAN-HAI-KUAN Station.

For general map of railway and specimens of bridges etc see plans T'ANG-KU to PEKING section.

China Office
21 January 1903

General 'with bullet'

Willing to print and publish
in China under British
Administration

The paragraph marked
on p. 1, which points
to the necessity of always
keeping a force at Wei-
hsien-tai.

The war office
have submitted proposal
B (see note in item 10 (b))
to the Treasury for
the

The distribution of our
force in the China for
the next 2 years

When evacuation of
W. China is decided

on, it would still be
desirable, to keep a
force at Wei-hsien-tai
to occupy the Railway
at once if necessary

The paragraph marked
on p. 30 shows the same
which would be followed
if necessary

shall

shall we ask for
another copy of the
report to send to
Peking.

Section

K. H.

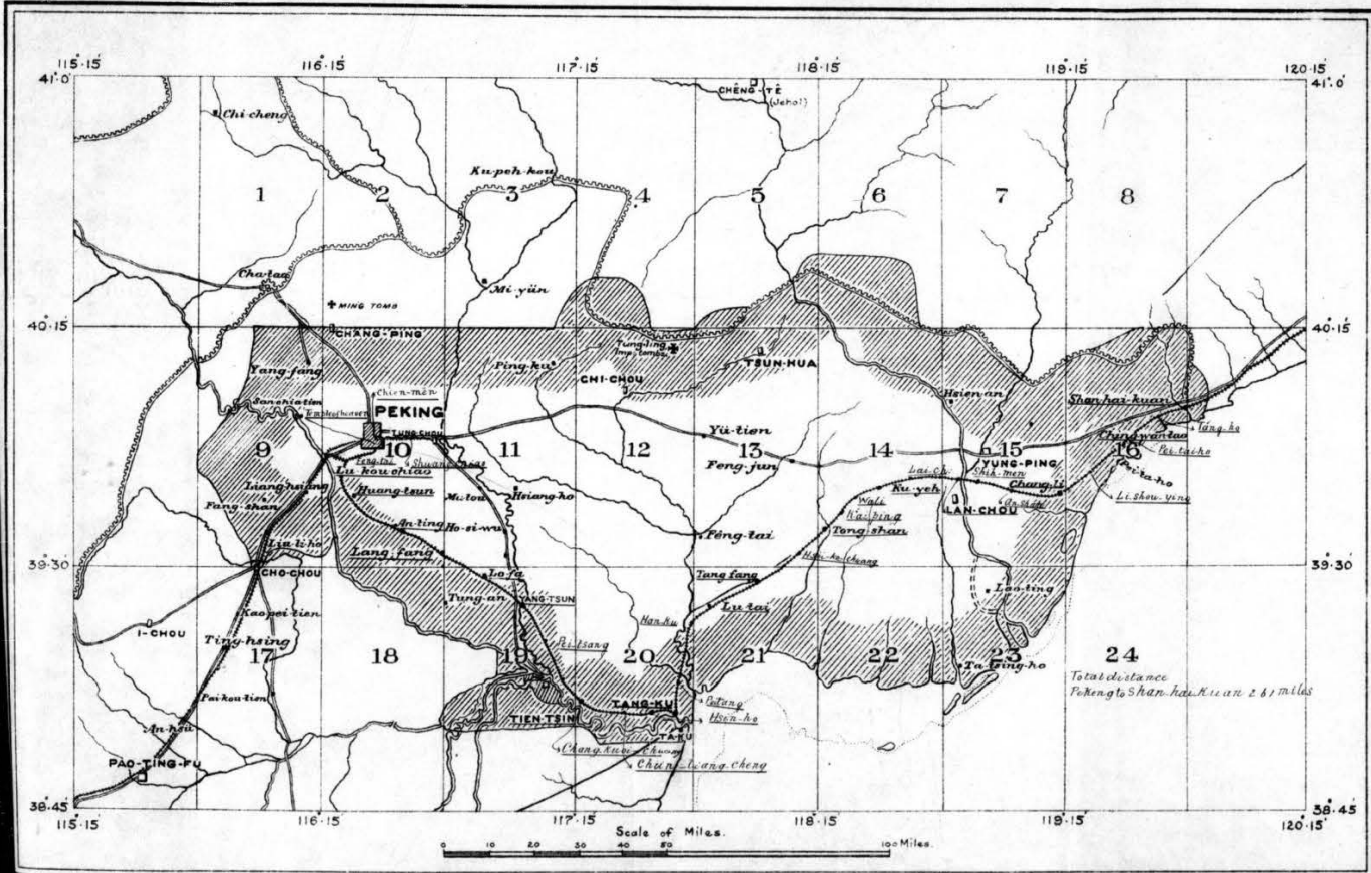
I. D. called for 2nd copy
Jan 21 1903

Reply they have not got
a copy. Copy returned in 1903

CHINA FIELD FORCE.

INDEX TO PRELIMINARY HALF-INCH MAPS

5



Reg. No. 190, 8, 1, 0—Aug. 0—1901.

Engraved at the Survey of India Office, Calcutta, August 1901.

No. 499-S. 01



WITH

Cultivated flat country
view limited by numerous
small willows and poplars
and orchards of apple
pear and plum trees

Cultivated flat country
view limited by numerous
small willows and poplars
and orchards of apple
pear and plum trees

Ho Hsi an

TIEN-TSIN

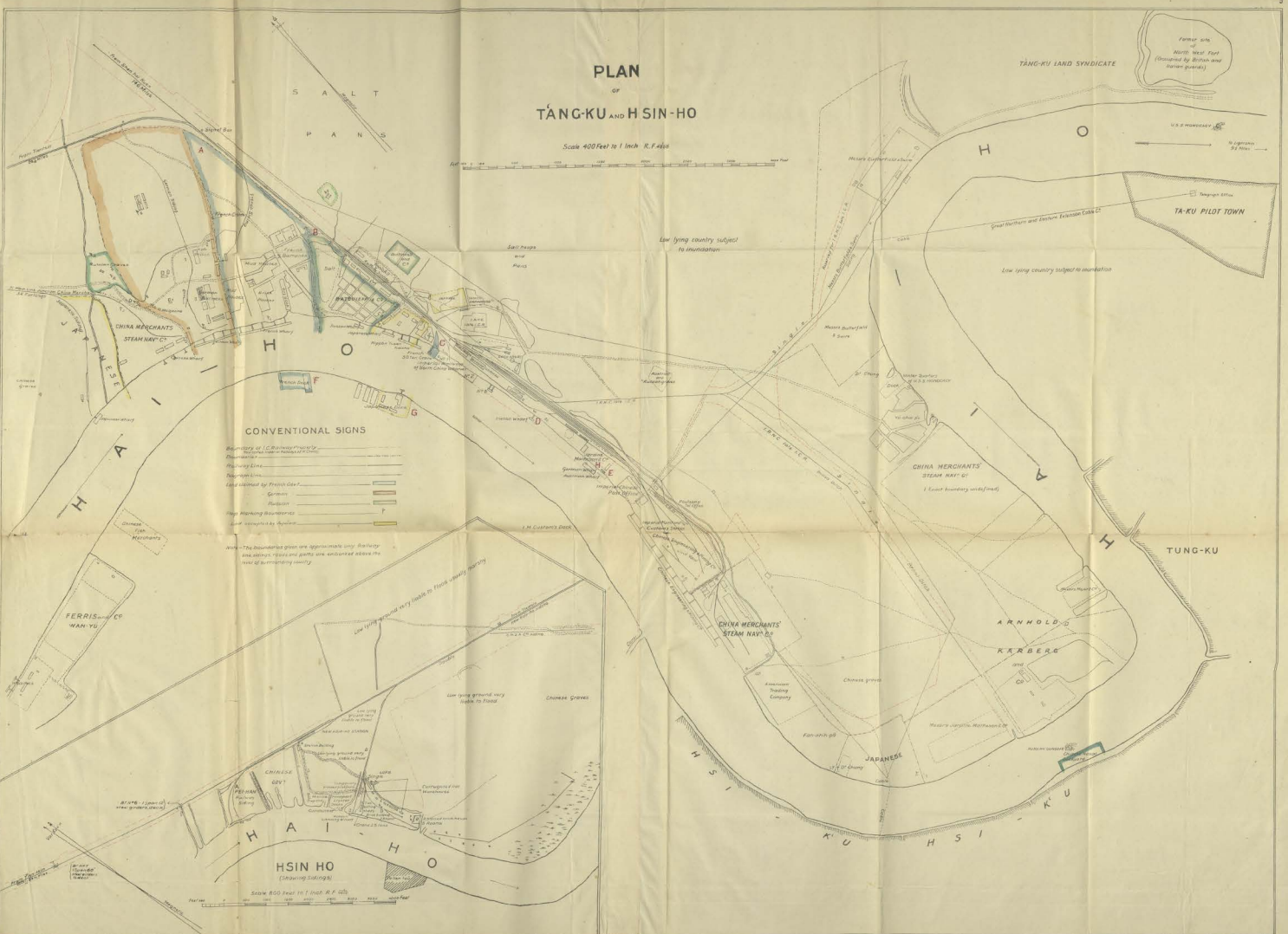
CHANG-KUEI-CHUNG

CHUN-LIANG-CHENG

NEIN-HO

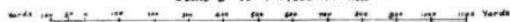
PLAN
OF
TANG-KU AND HSIN-HO

Scale 400 Feet to 1 inch R.F. also



and one mile round

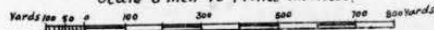
Scale 6" to 1 Mile R.F. 10/20/20



and one Mile round

Scale 6 inch to 1 mile N.P. 10560.

Yards 100 50 0 100 300 500 700 800 Yards



BEN#1 Span 90 $\frac{1}{2}$ through

→ GENIS 6 spans 30 deck

- To Táng-ku 18½ miles

PLAN OF CHANG-KUEI-CHUANG STATION

Scale 200 Feet to 1 inch

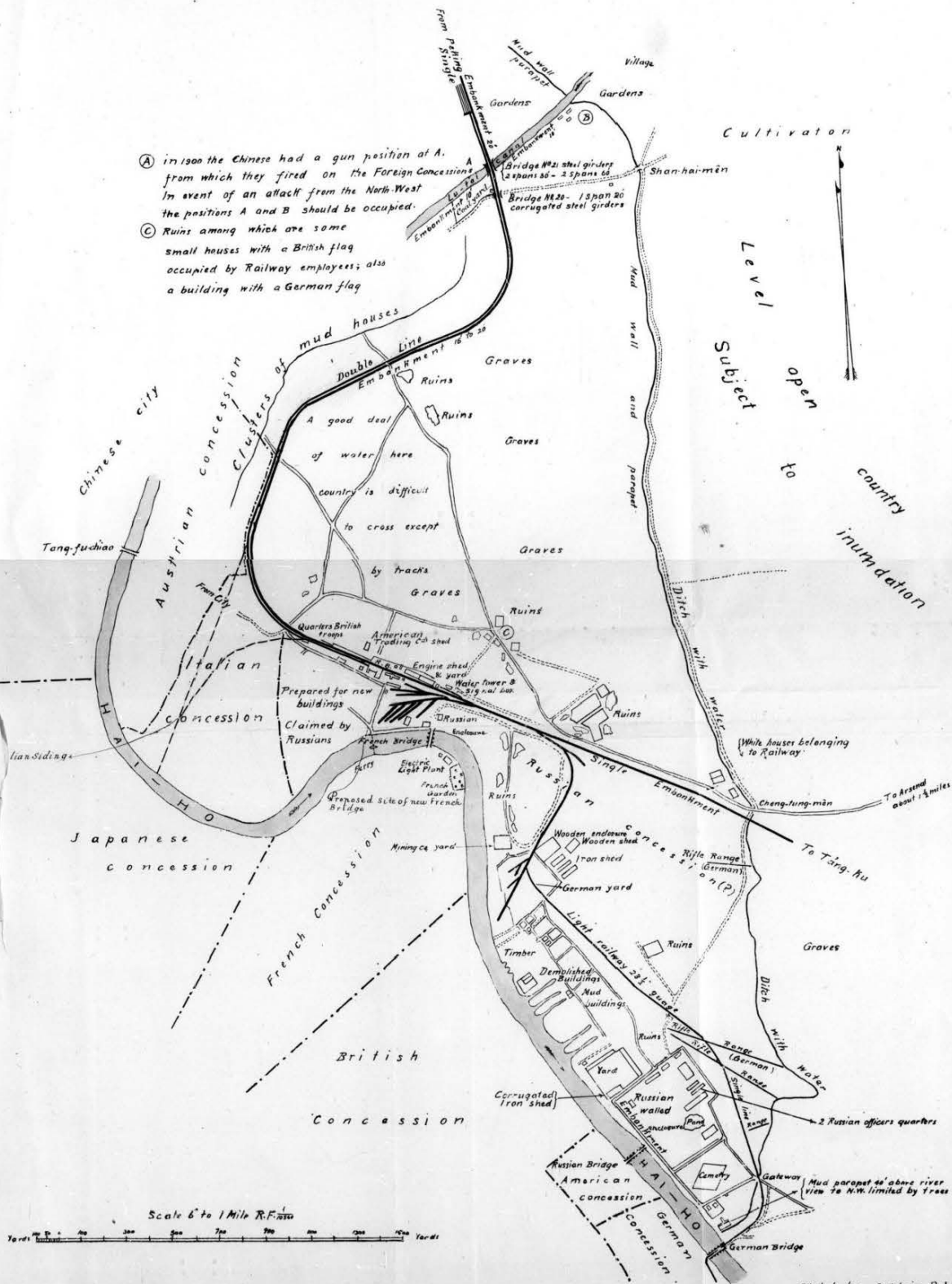


To Táng-ku $18\frac{3}{4}$ Miles

天津 TIENTSIN RAILWAY STATION

and neighbourhood

- (A) in 1900 the Chinese had a gun position at A, from which they fired on the Foreign Concessions. In event of an attack from the North-West the positions A and B should be occupied.
- (C) Ruins among which are some small houses with a British flag occupied by Railway employees; also a building with a German flag

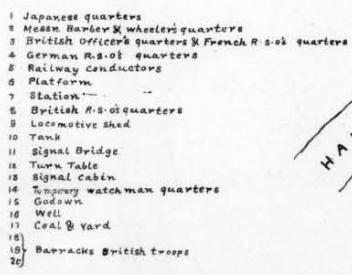


Sketch by Captain P.J. Miles

4th P.I.

January 1902

13



北
PEI-TSANG STATION
and one mile round

14

Ground lately
under water

Inundated Land

Temporary shelter for troops
Shelter for German Patrol

Shed

Shed

Inundated Land

Station and Quarters

Shelter for
German Patrol

Shed
□
HOUSE

Scale 100 Feet to 1 inch

R.F. 1/100

Feet 100 50 0 50 100

Scale 6 inches to 1 Mile

R.F. 1/2500

Yds 100 50 0 1 2 3 4 5 6 7 8 9 10 110 Yards

Japanese Telegraph line
French Telegraph line
British Telegraph line
From Yenchow to Wei Wei
American Telegraph Line
Private Cog Line

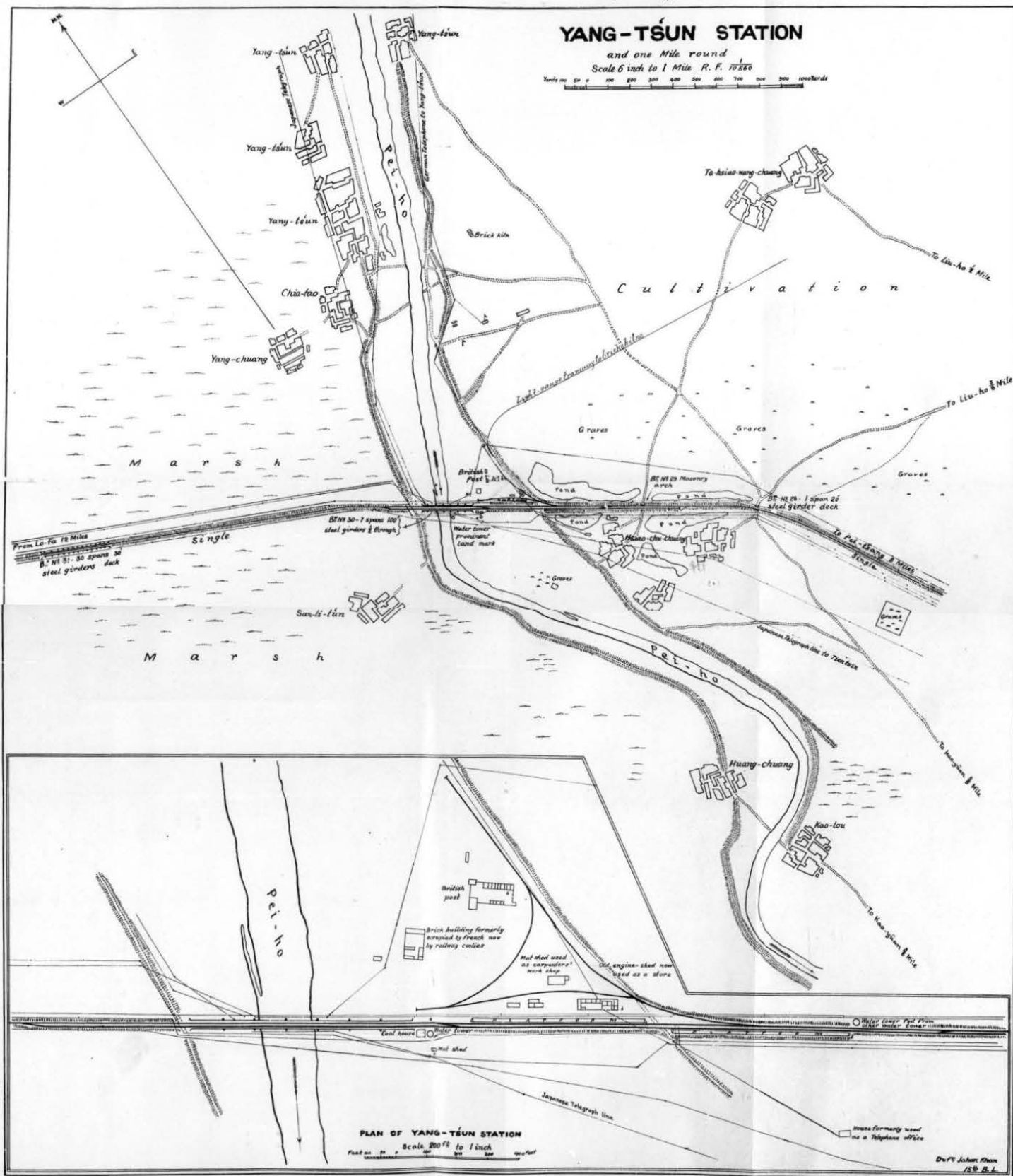
C. M. Carter Capt.
14 Litch

村 楊

YANG-TSUN STATION

and one Mile round

Scale 6 inch to 1 Mile R. F. $\frac{1}{10880}$



PLAN OF YANG-TSUN STATION

Scale 200 ft to 1 inch

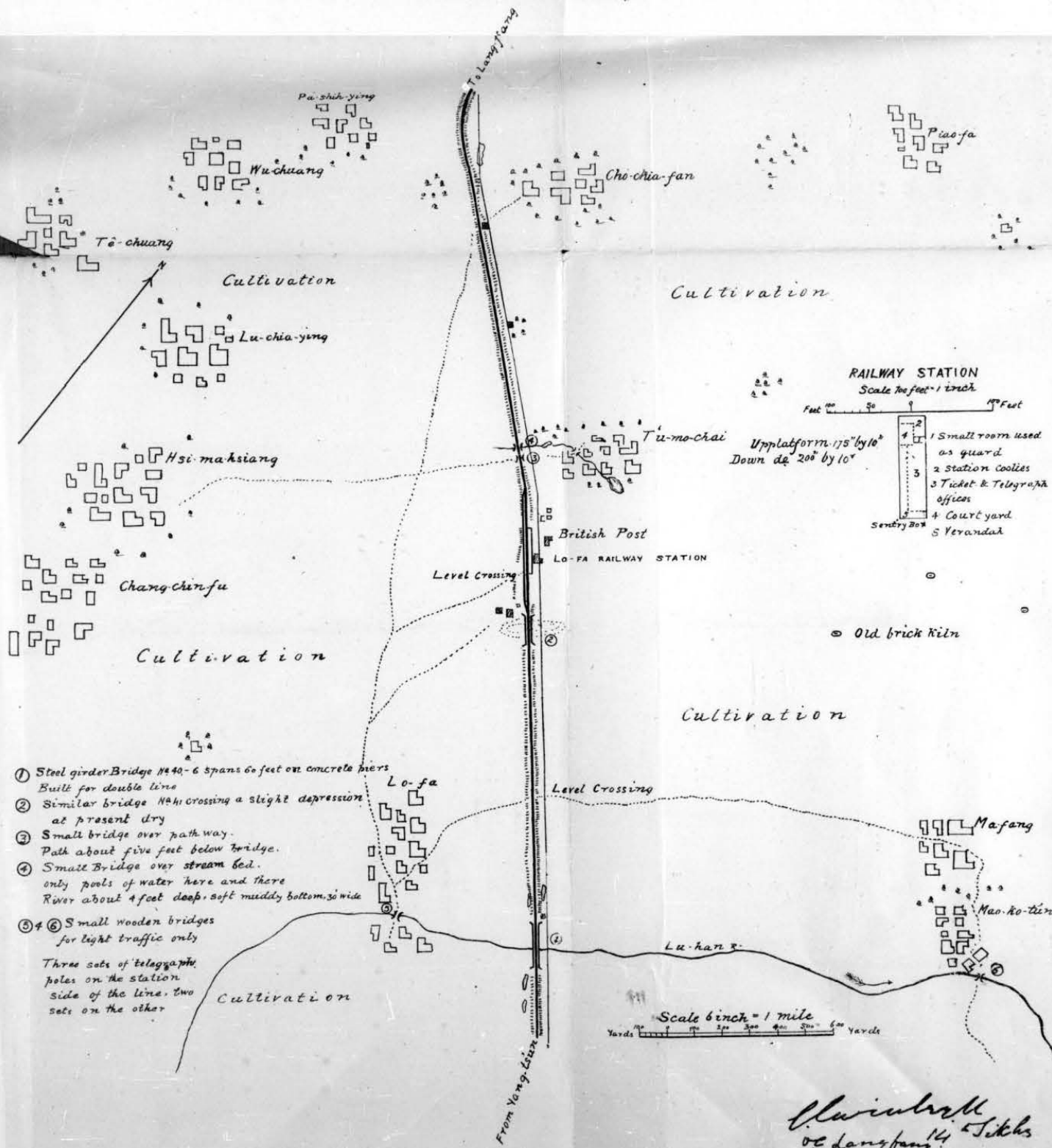
feet in 1000 ft. 1000 ft. 1000 ft. 1000 ft. 1000 ft.

References

LO-FA STATION

16

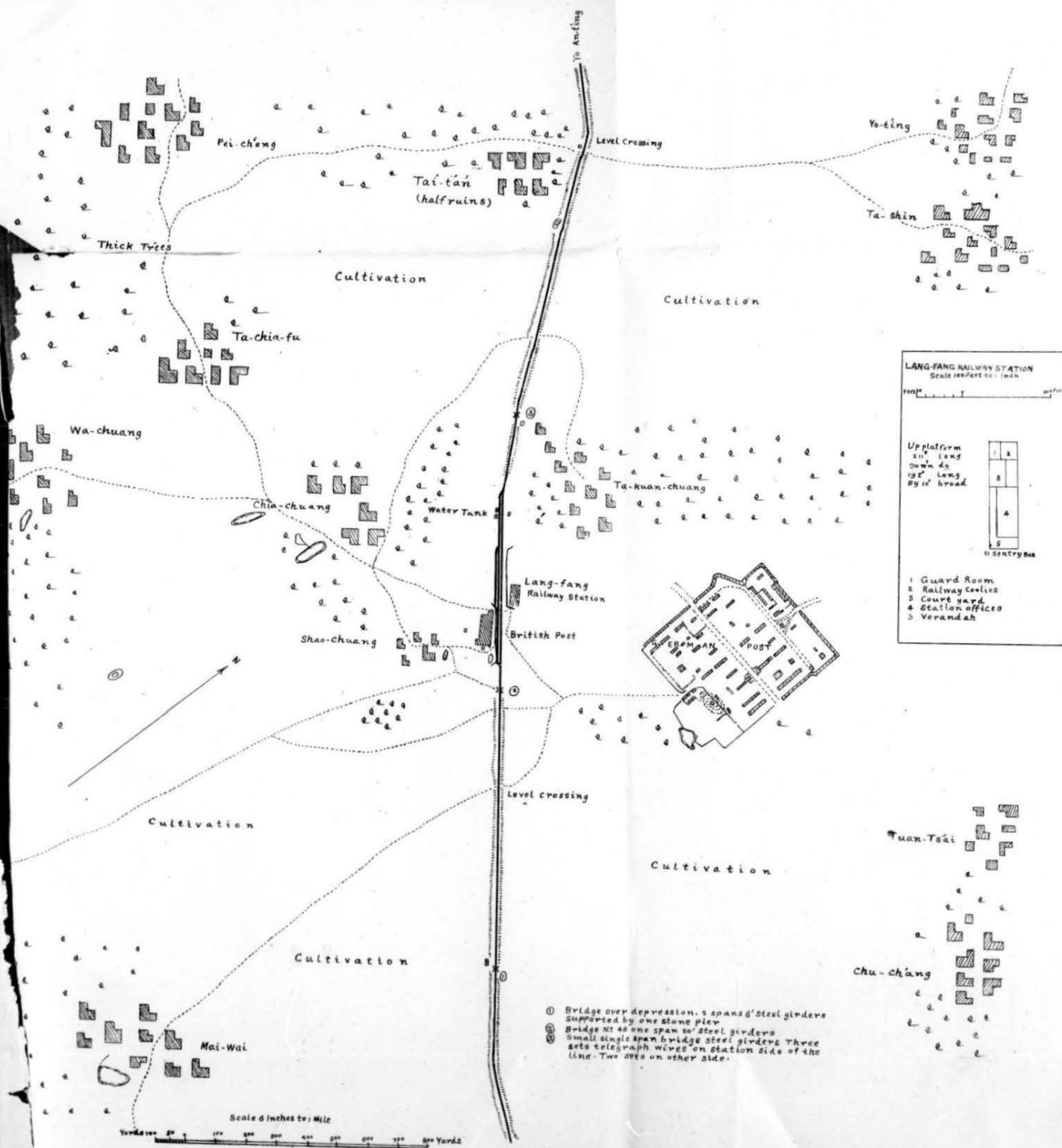
and one mile round



坊郎

LANG-FANG STATION

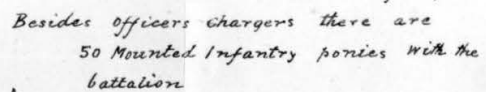
and one mile round



From L. O. F.

Chwibrell
14. L. O. F. Lang-fang

Open Ground

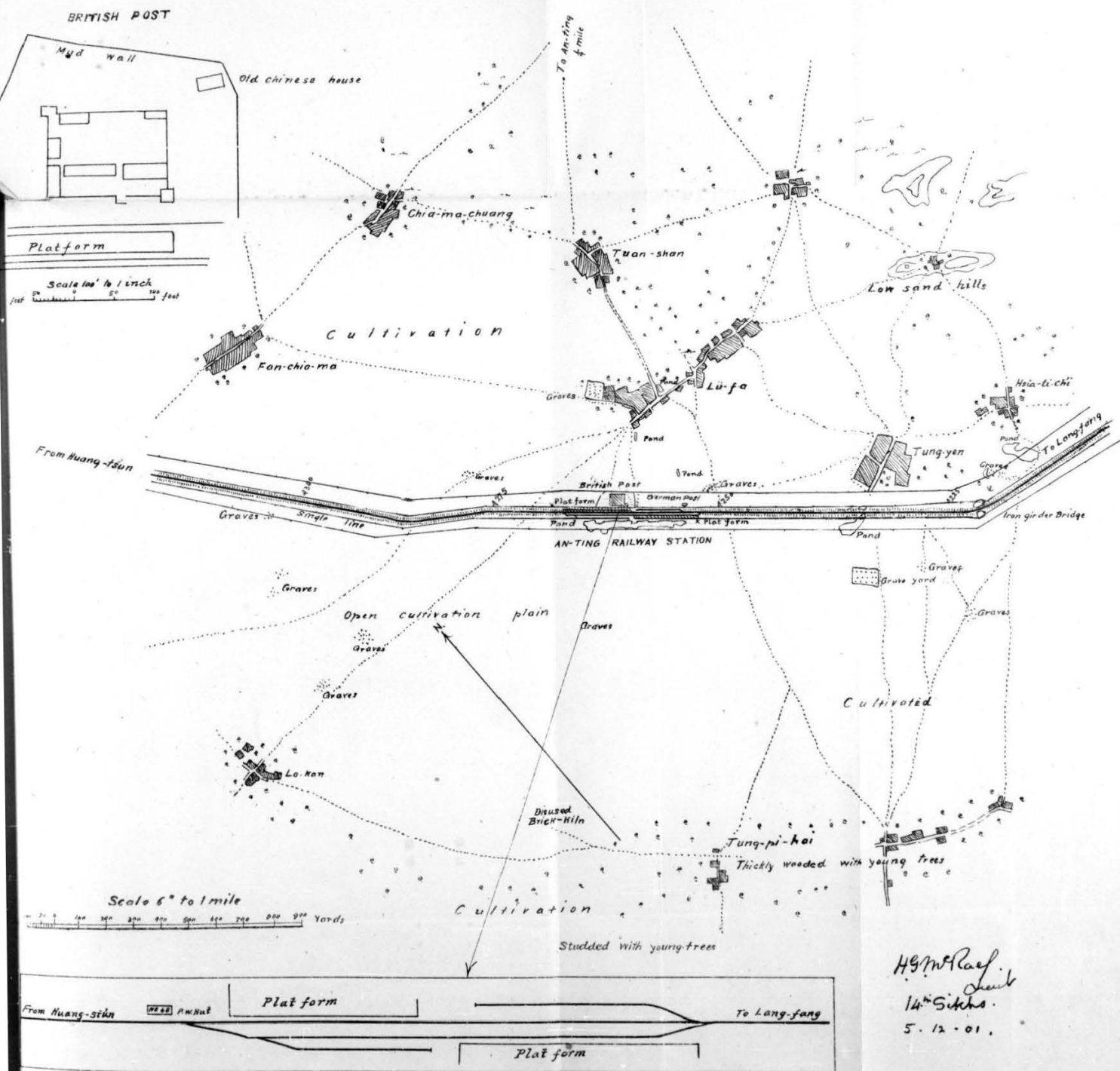


Frq. Jambot dient
14th Jikhs

6-1-02

定安 AN-TING STATION

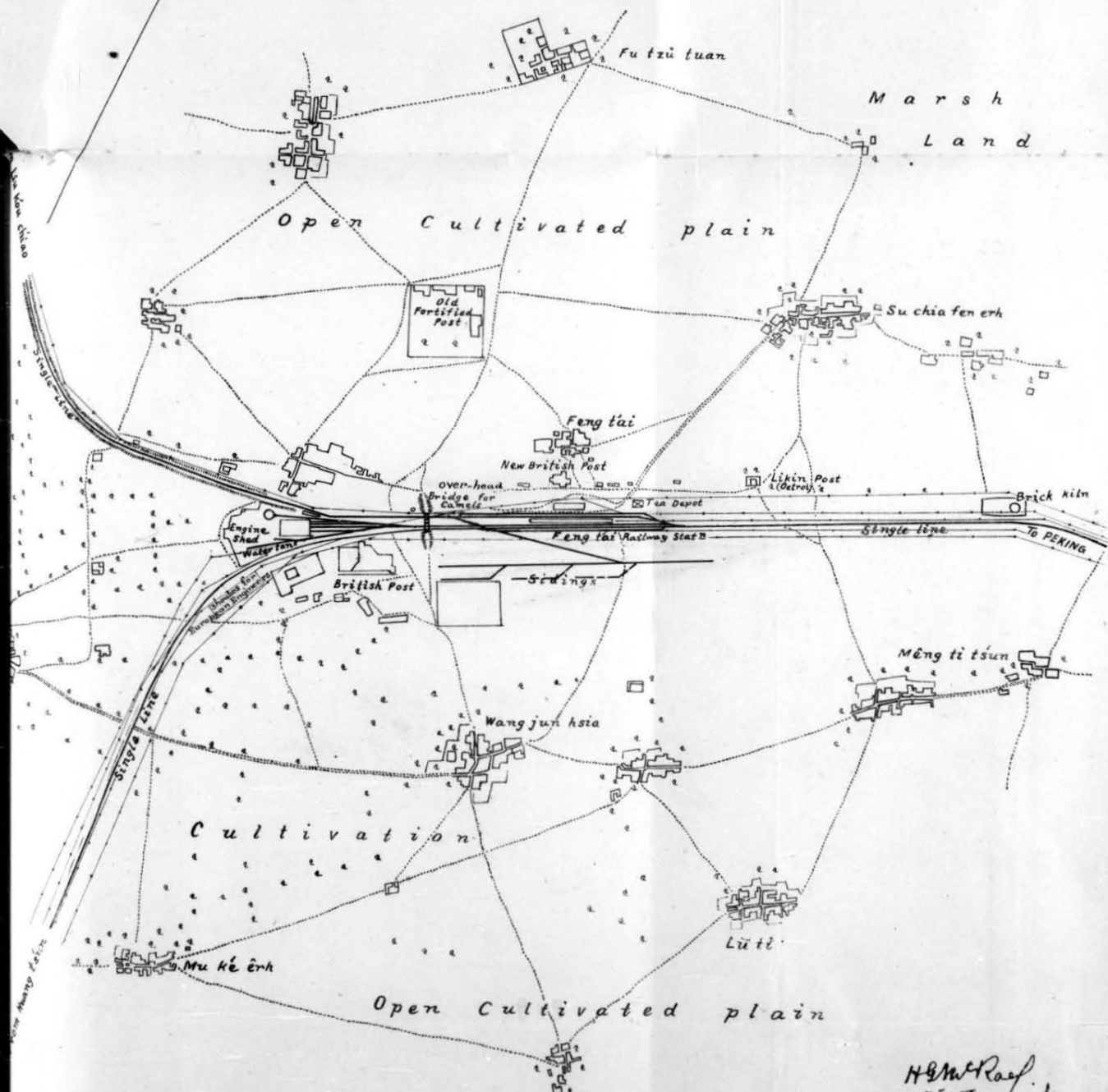
and one mile round



台 豐

Country 1 Mile round

FENG TAI RAILWAY STATION



Scale 6" to 1 Mile

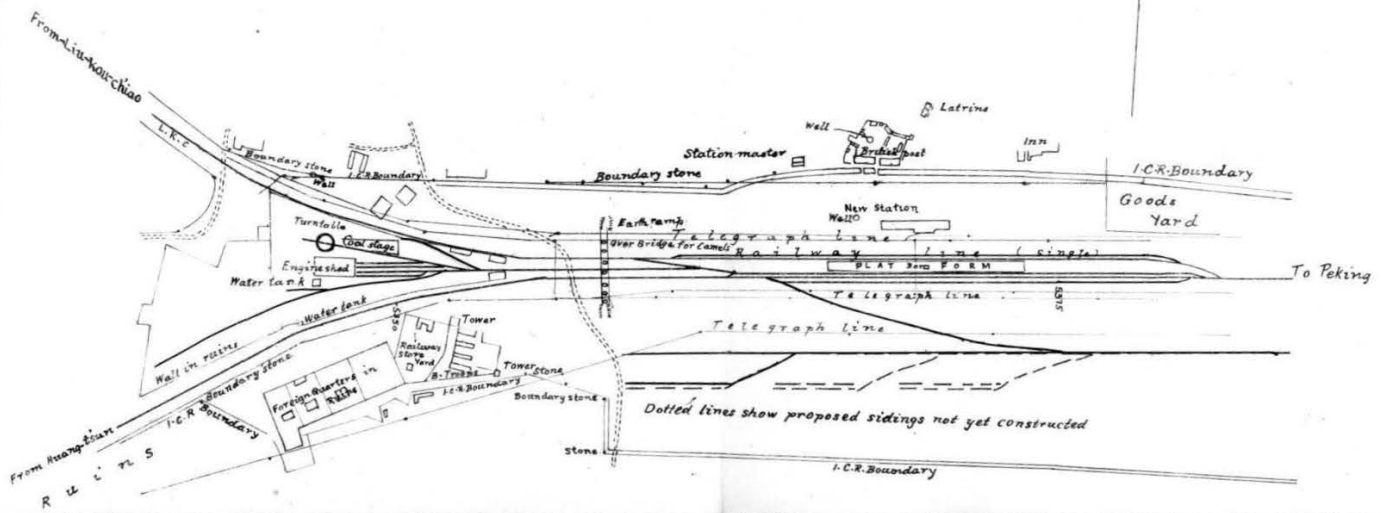
Yards 100 200 300 400 500 600 700 800 900 1000 Yards

H. B. M. R. R. R.
Lieut.
14th Sikhs
6.12.01.

台 豐 FENG TAI STATION AND POST

22

Scale 400 Feet = 1 Inch
From B.R.A. plans



壇天

PLAN OF TEMPLE OF HEAVEN STATION

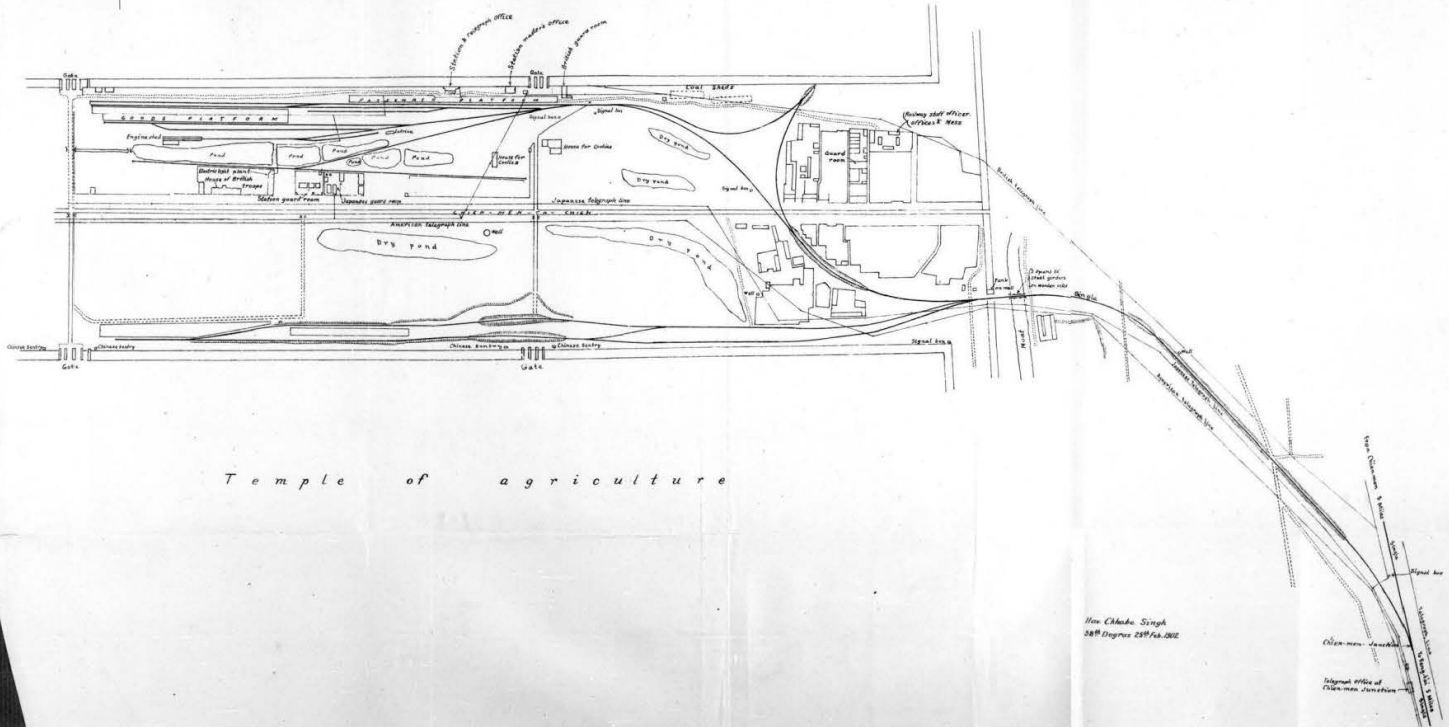
and line from Chien-men Junction

Scale 200 ft to 1 inch A.C. 1902

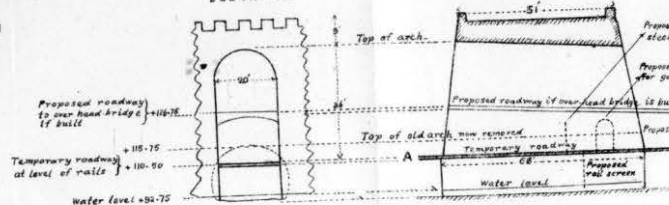
Feet 0 100 200 300 400 500 600 700 800 900 1000

23

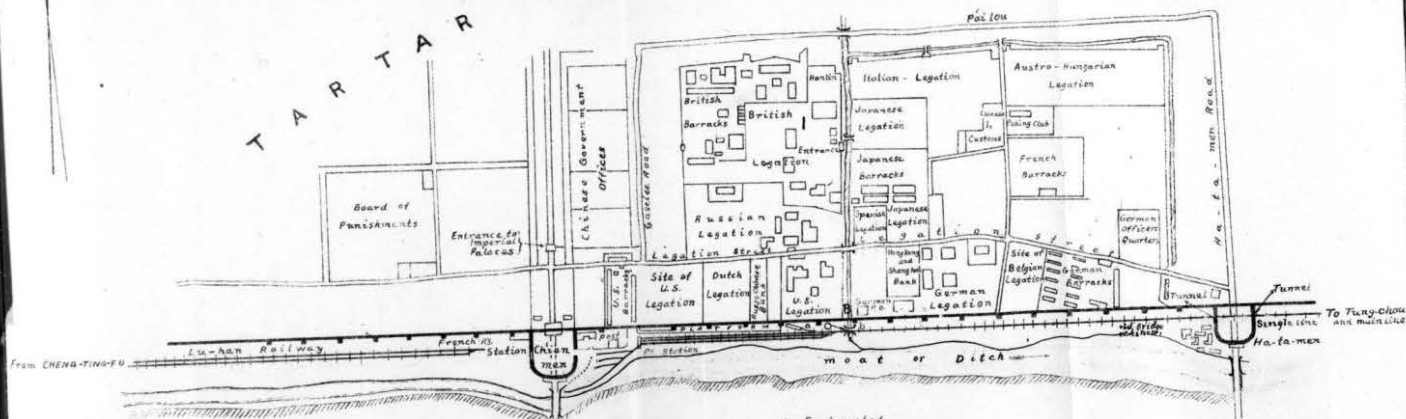
Temple of Heaven



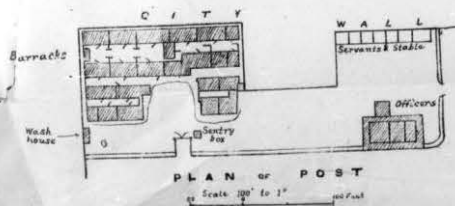
SALLY - PORT at WATER-GATE
ELEVATION SECTION on A.....B



C I T Y



- a. Engine shed
- b. Water Gate Now being made in to a salty port

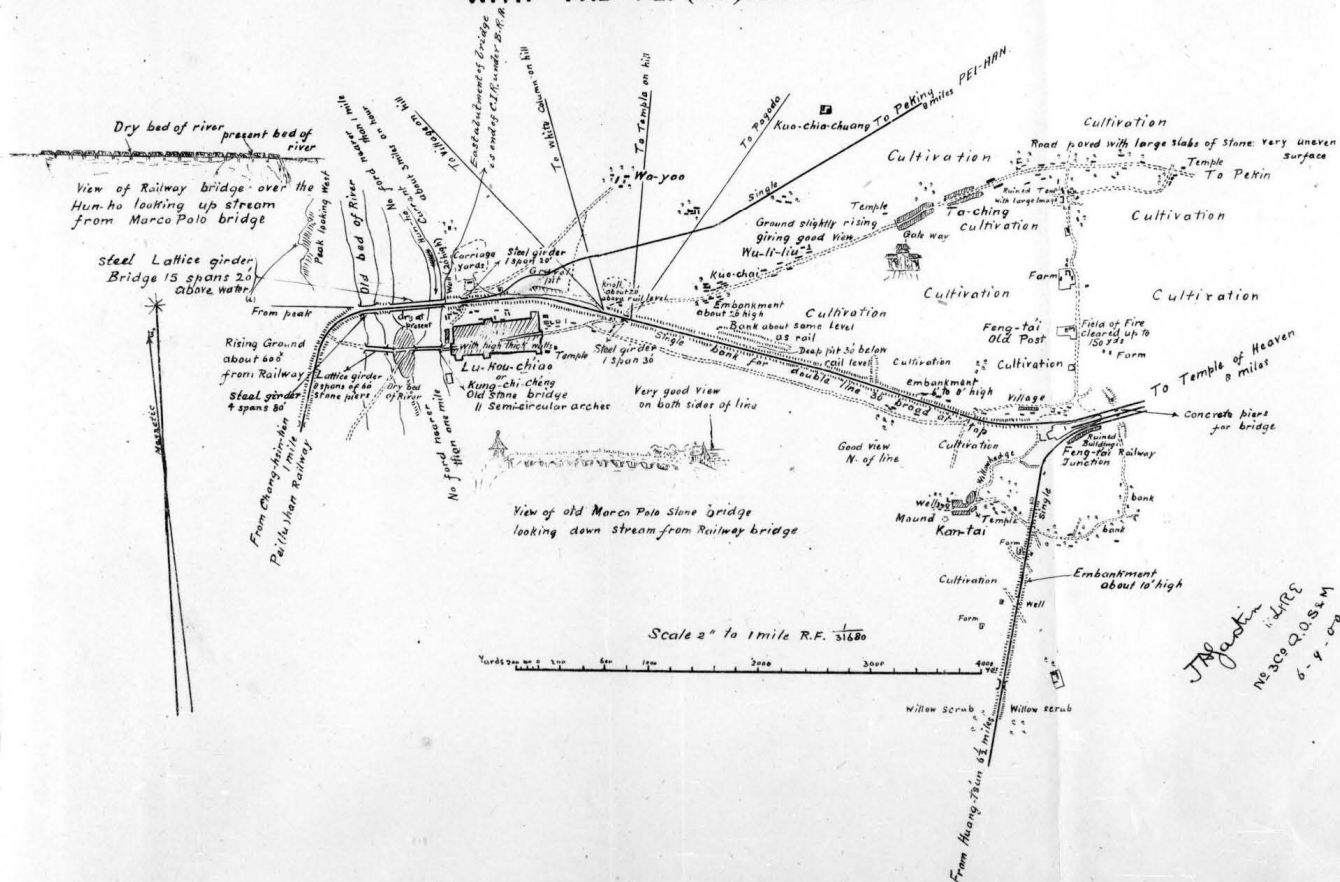


Scale 6" to 1 Mile

Capt. R. M. Parfitt
attd. 14th Schhs

蘆溝橋

LU-KOU-CHIAO EXTENSION SHOWING JUNCTION
WITH THE PEI (LU) HAN RAILWAY

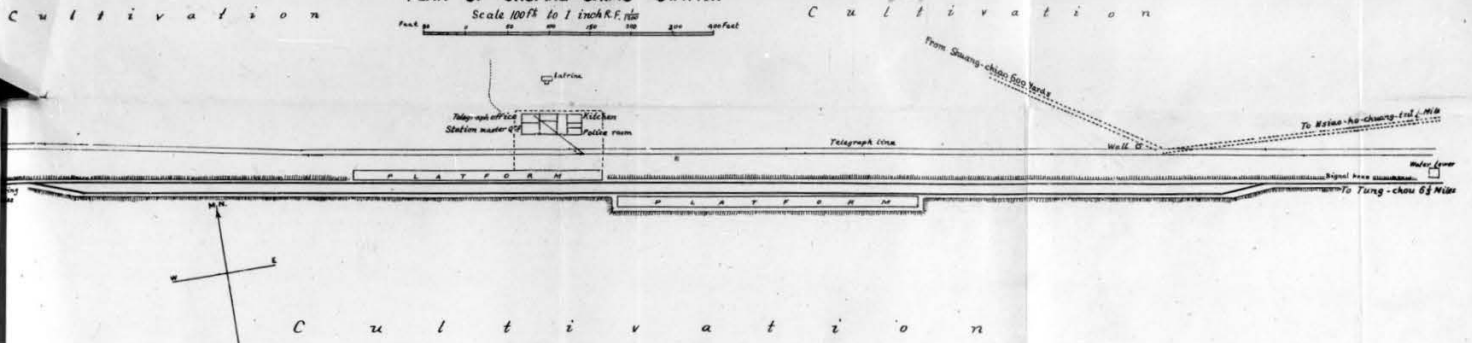


橋 雙

27

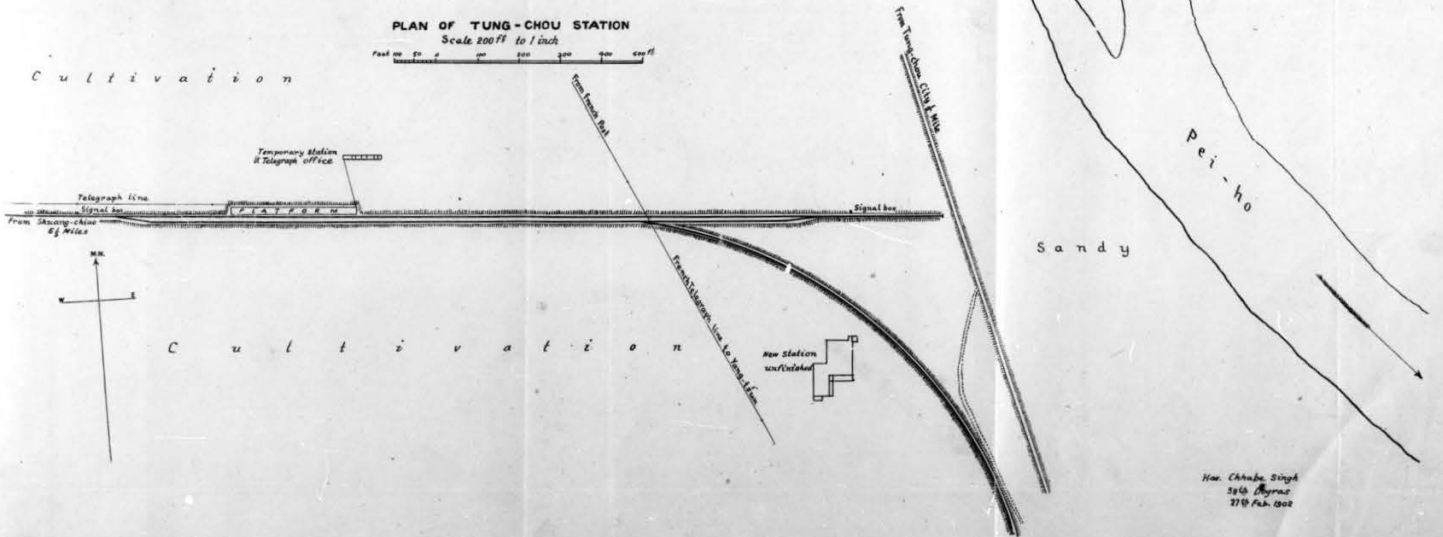
PLAN OF SHUANG-CHIAO STATION

Scale 100 ft to 1 inch R.F. 1/100



PLAN OF TUNG-CHOU STATION

Scale 200 ft to 1 inch

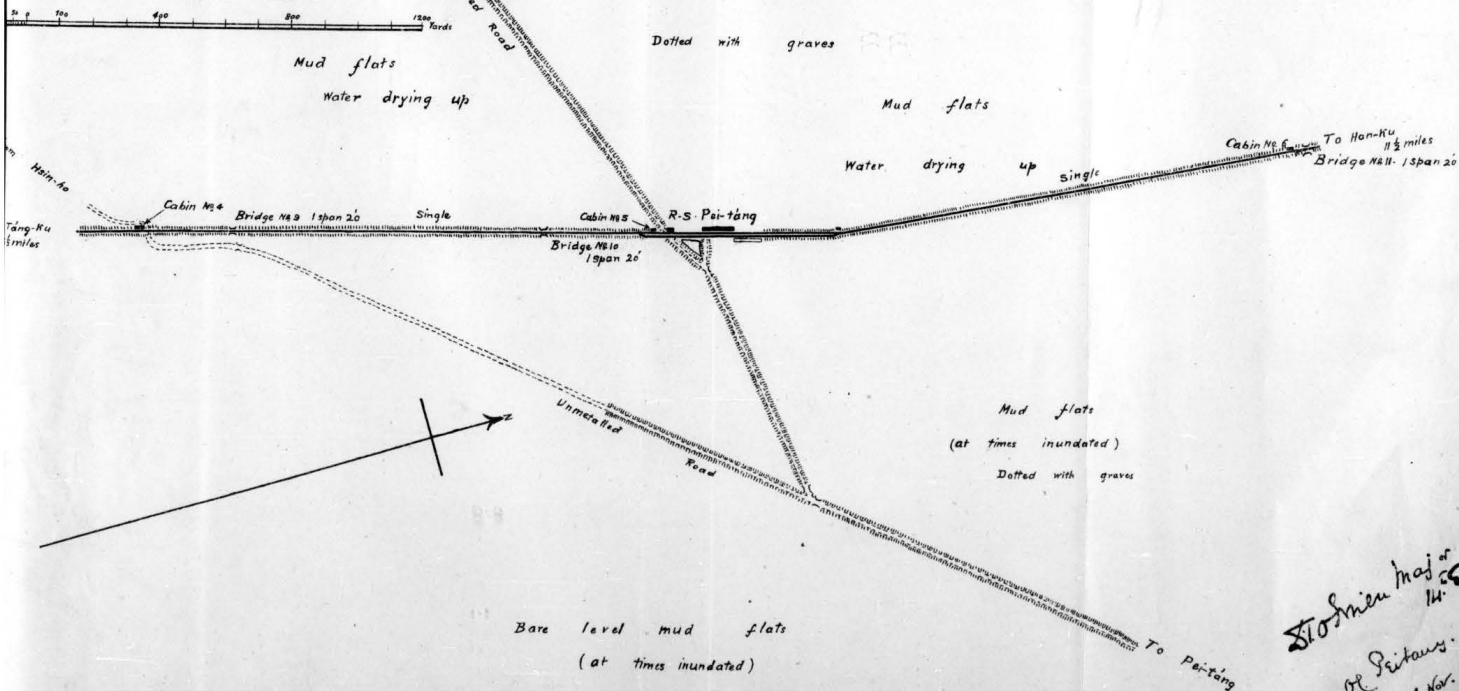
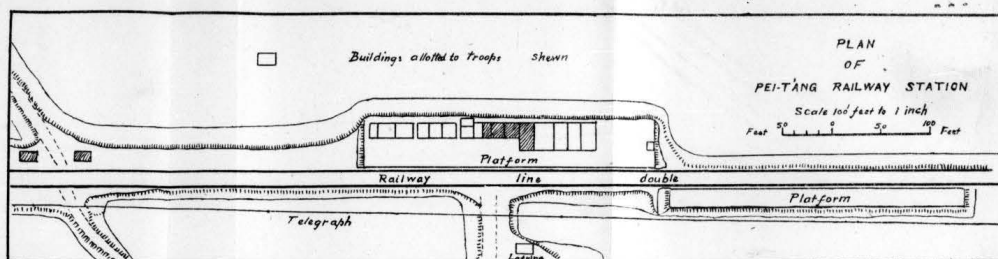


塘北

PEI-T'ANG STATION

and one mile round

Scale 6" to 1 mile R.F. 1/25000



Station Major
14.5 kts.
at Pei-t'ang.
23rd Nov. 1914.

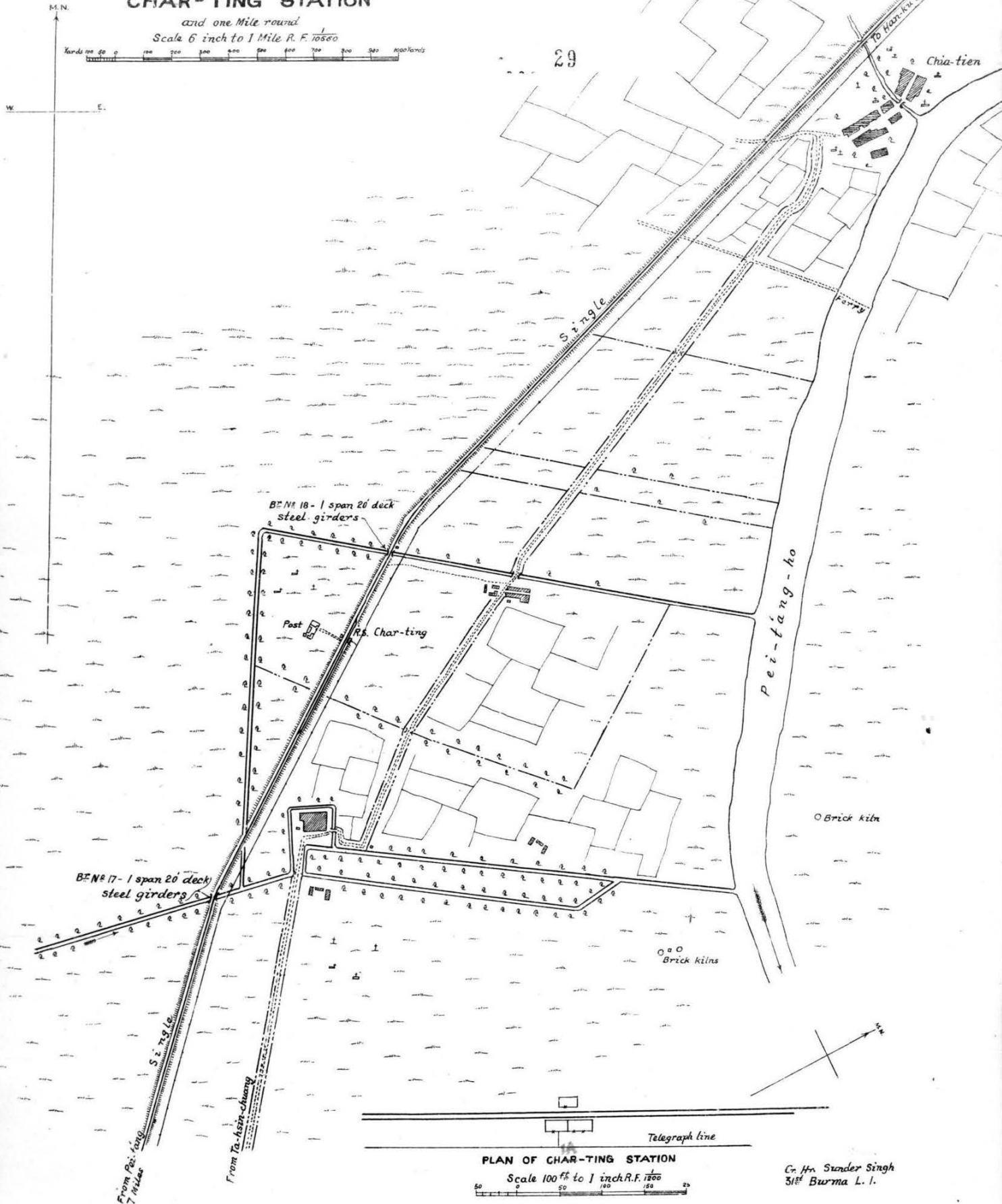
CHAR-TING STATION

and one mile round

Scale 6 inch to 1 Mile R.F. 10560

Yards 0 100 200 300 400 500 600 700 800 900 1000

29



PLAN OF CHAR-TING STATION

Scale 100 ft to 1 inch R.F. 10560

50 100 150 200

By Hn. Sunder Singh
31st Burma L. I.

沽漢

HAN-KU STATION

and one Mile round

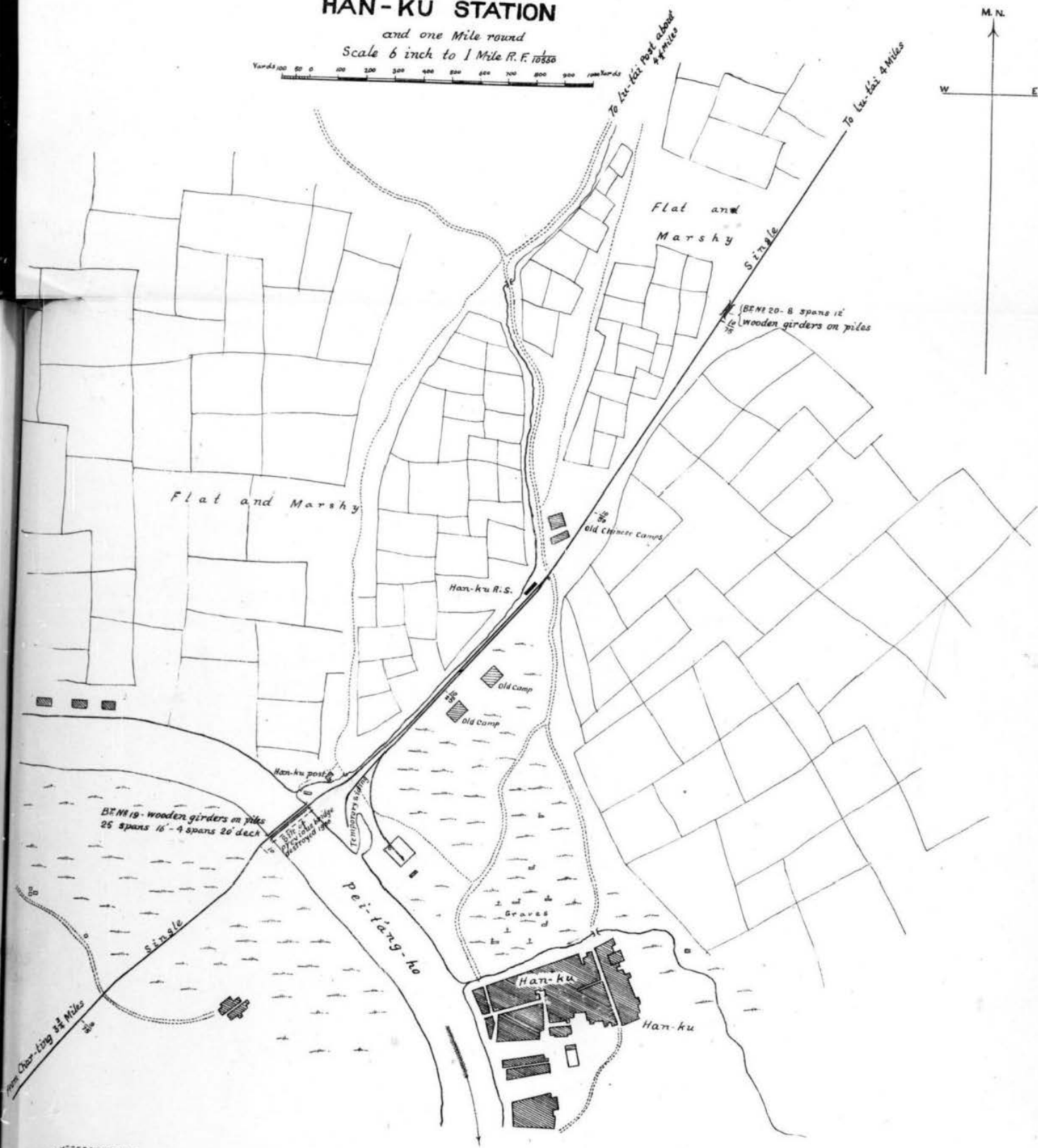
Scale 6 inch to 1 Mile R.F. 10550

Yards 100 200 300 400 500 600 700 800 900 1000

30

M. N.

W E



PLAN OF HAN-KU STATION

Scale 100 ft to 1 inch R.F. 1/1250

T. Timbrell. 16

O.C. Han-ku post

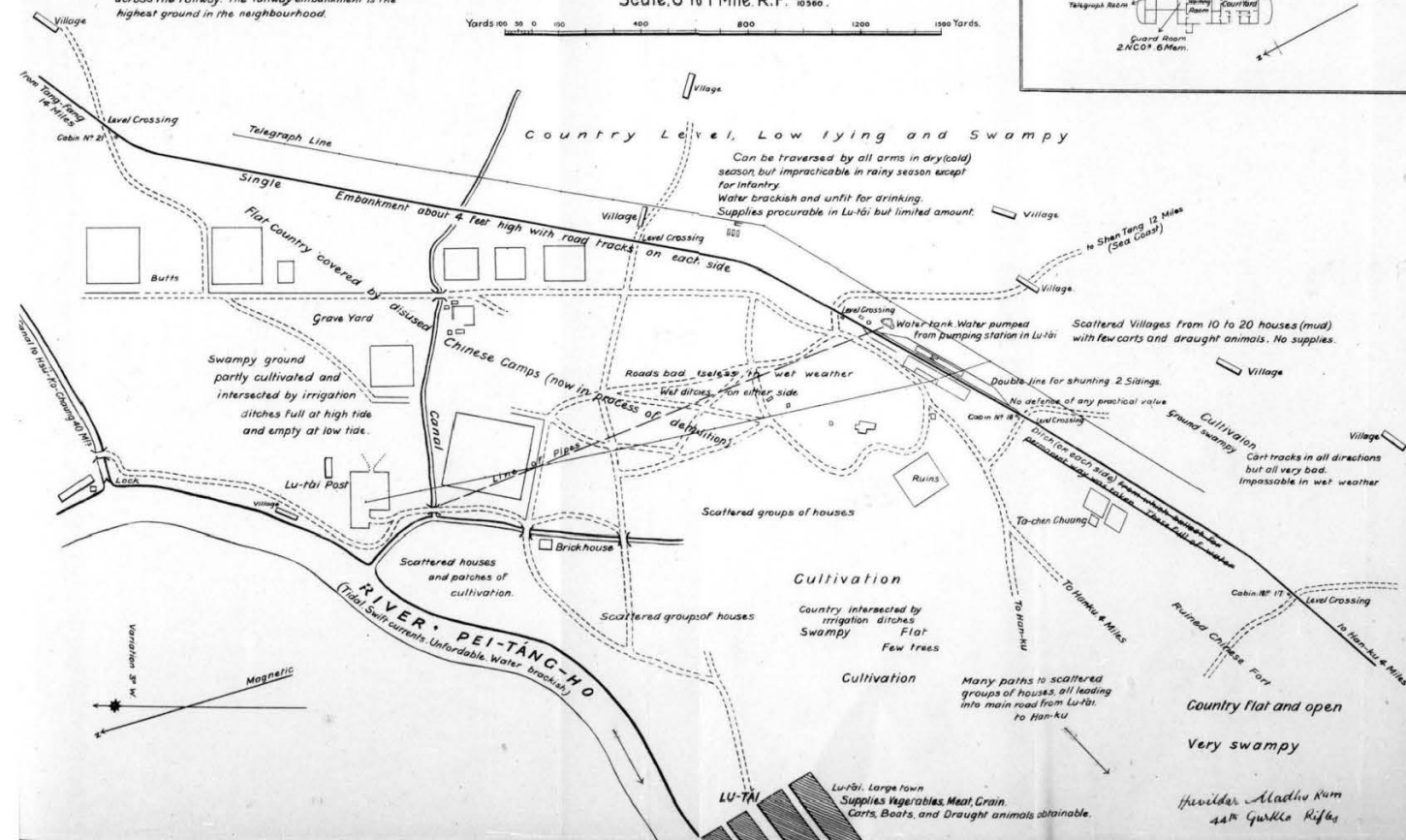
PLAN OF LU-TAI STATION

Scale 100 Feet to 1 Inch

Age Group	Percentage
18-24	10%
25-34	15%
35-44	20%
45-54	25%
55-64	20%
65-74	15%
75-84	10%
85+	5%

A hand-drawn floor plan of a guard room and its surroundings. The plan shows a rectangular building with several rooms. A 'Platform' is located at the top. To the left of the building is a 'Telegraph Room'. Inside the building, there is a 'Waiting Room' and a 'Court Yard'. Below the building is a 'Guard Room' with '2 NCOs' and '6 Men'.

31



坊唐

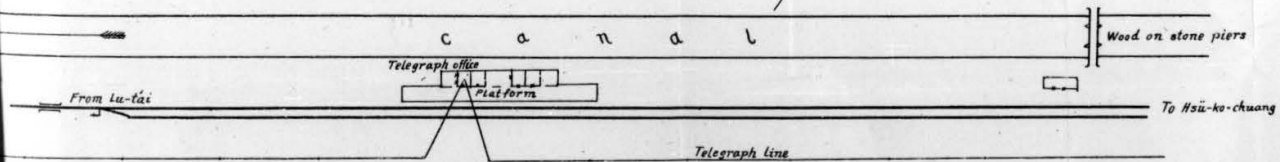
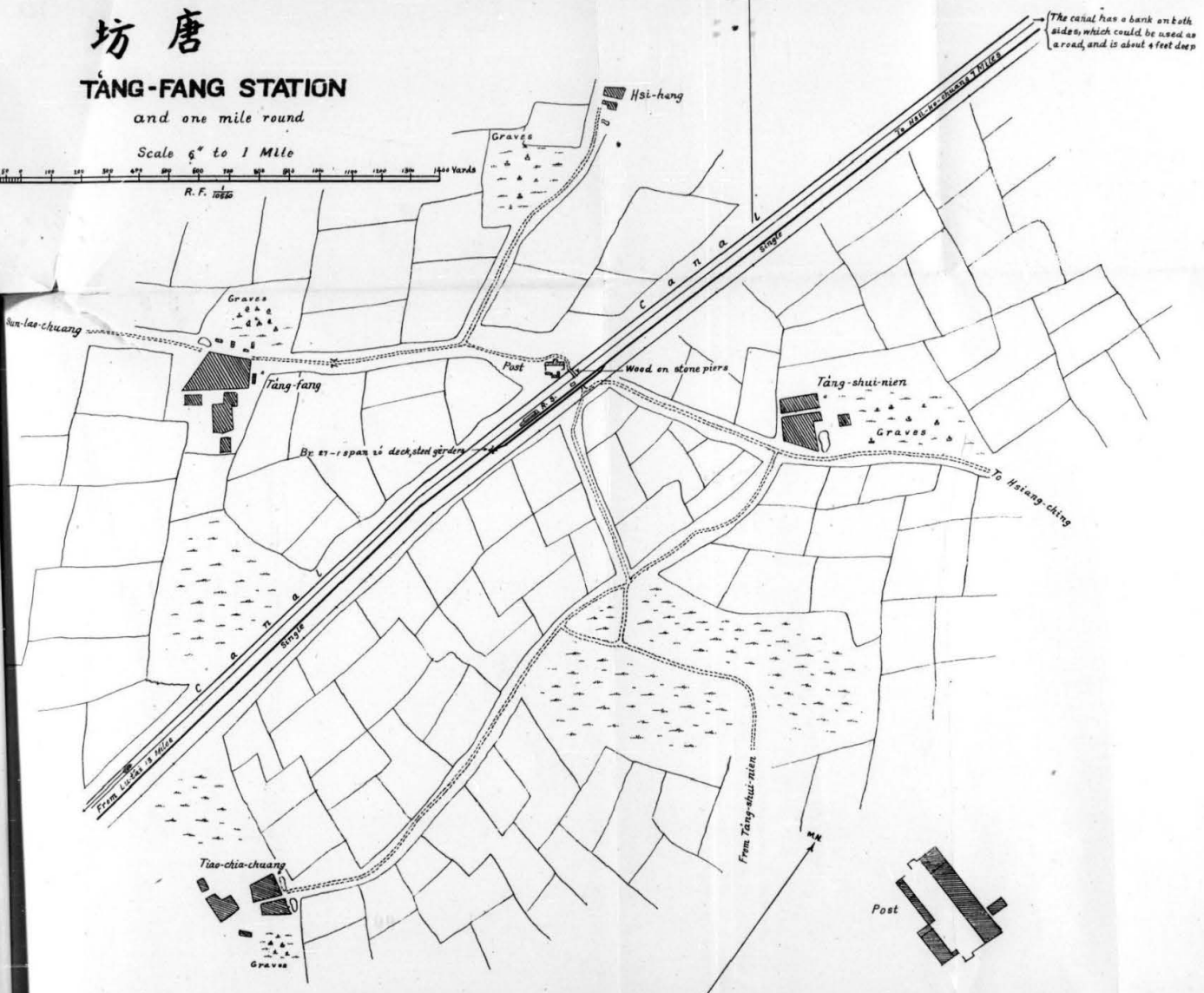
TANG-FANG STATION

and one mile round

Scale 6" to 1 Mile

R.F. 1885

Yards 100 200 300 400 500 600 700 800 900 1000 1100 1200 1300 1400 1500



PLAN OF TANG-FANG STATION

Scale 100 feet to 1 inch

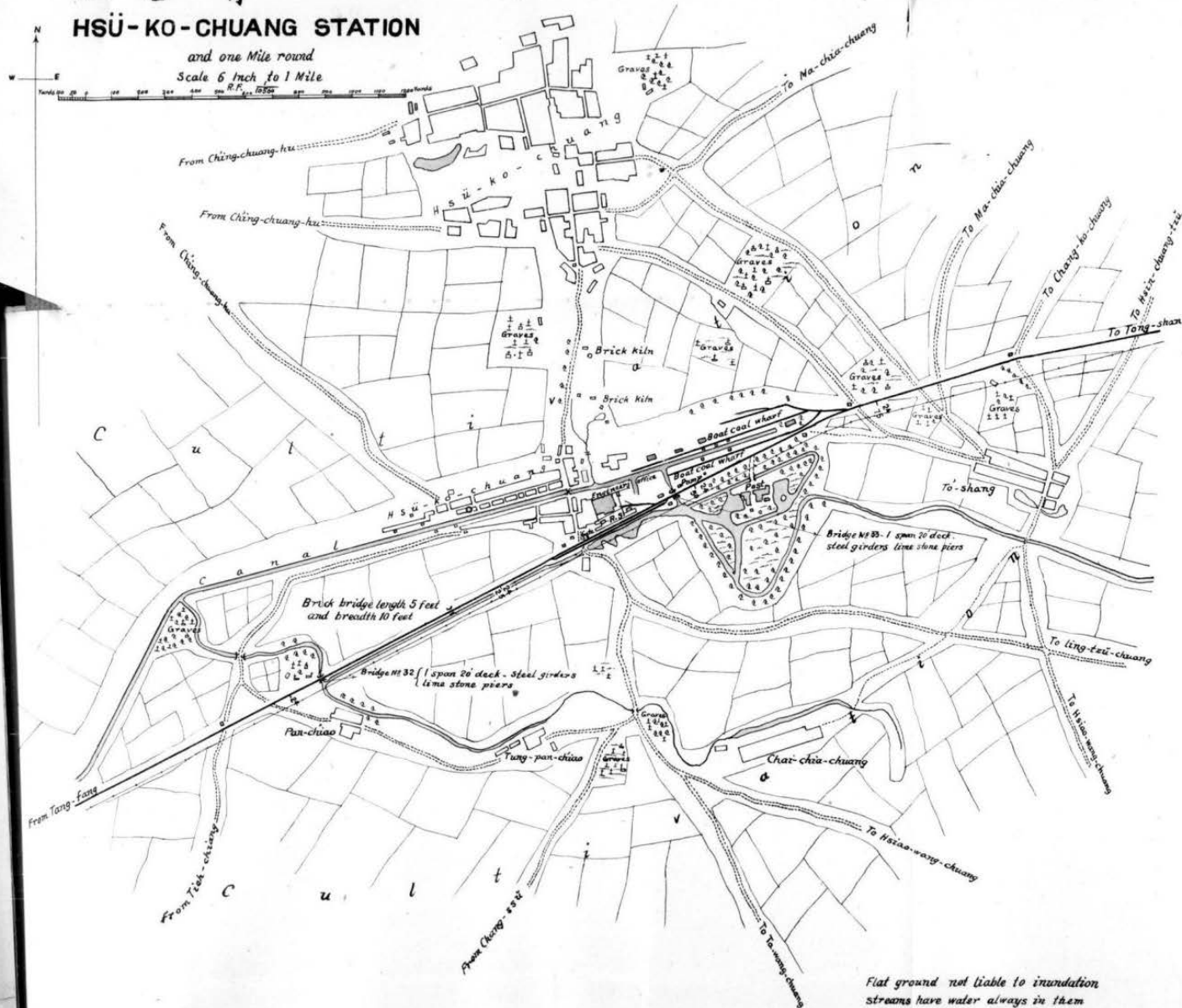
Feet 100 200 300 400 500 600 700 800 900 1000

Ex. Hm. Sundar singh
S. & Burma. L. I

庄各骨

HSÜ-KO-CHUANG STATION

and one Mile round
Scale 6 inch to 1 Mile



PLAN OF HSÜ-KO-CHUANG STATION



C. H. Sundar Singh
Jell. Burma L. I.

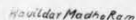
and One Mile Round

Scale 6 inches to 1 Mile R.F. 1:316,800



Scale 200 feet to 1 inch

feet 100 50 0 100 1 3 4 5 6 7 8 100 feet

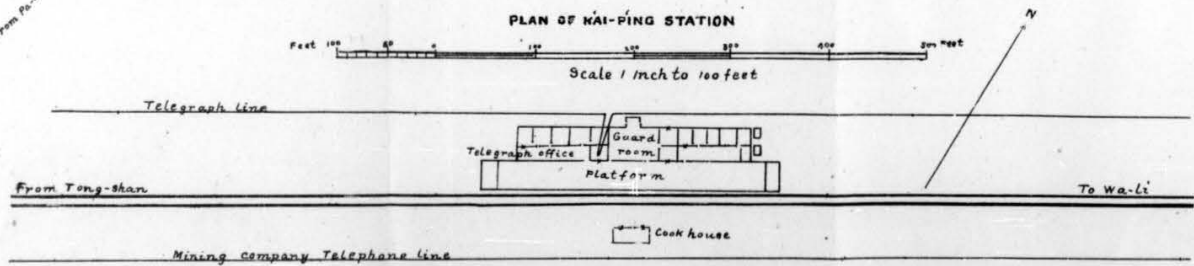
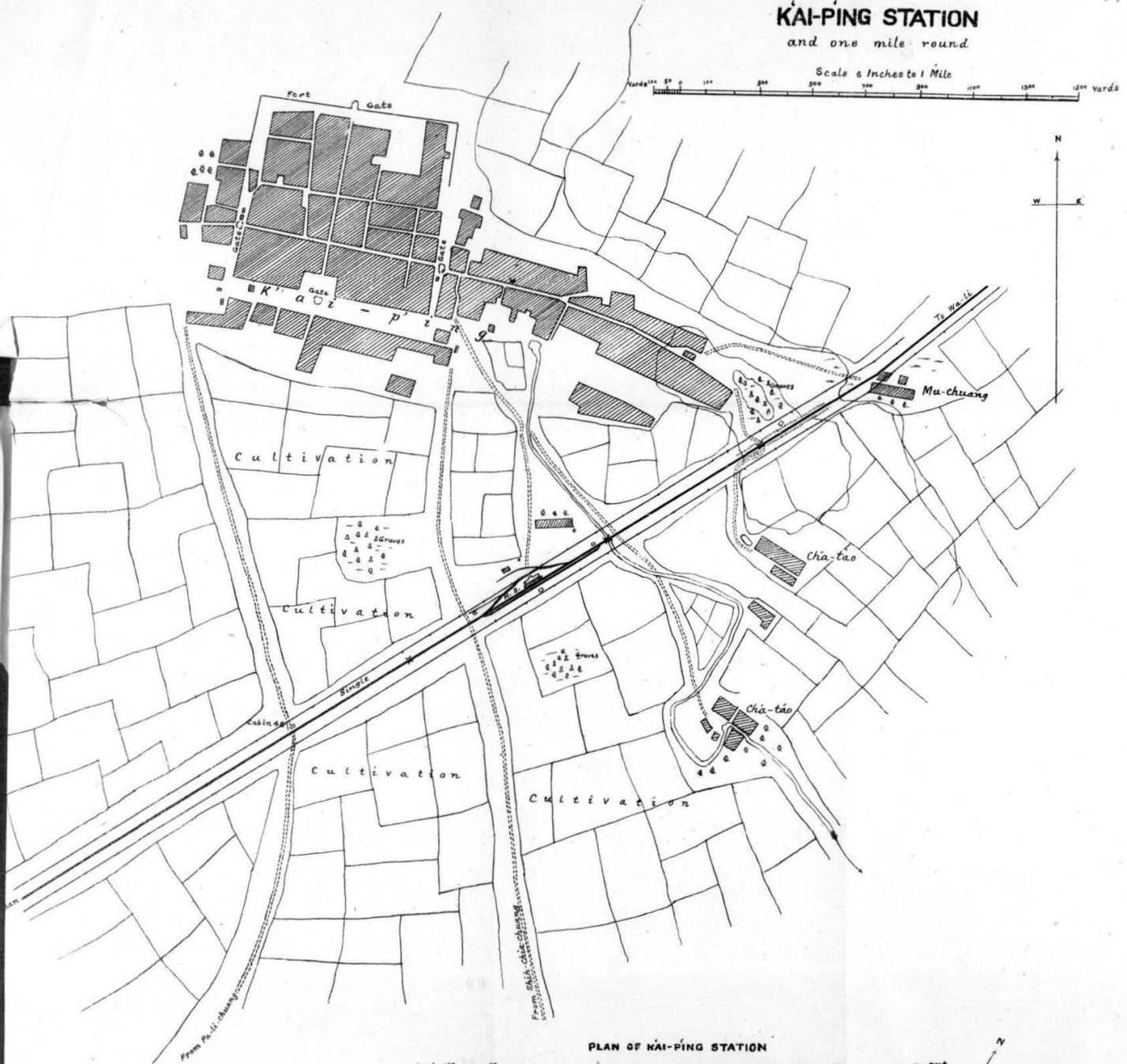


••• Gurkhas

Litho B.S.L.M. China Force 8-10-02 (30) WJ

平開 KAI-PING STATION and one mile round

35



C. H. Sundar Singh
6" Burma L.I

里窪

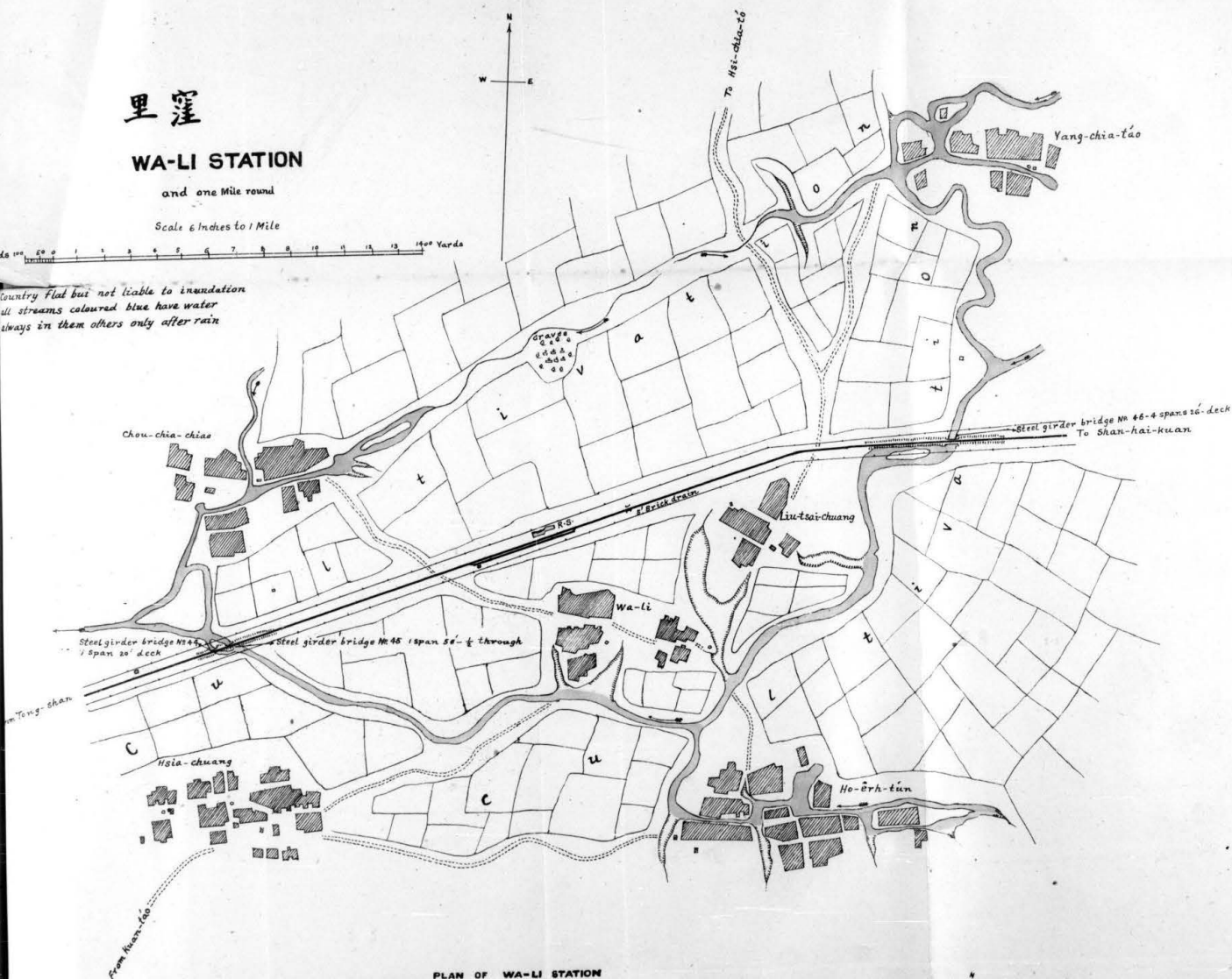
WA-LI STATION

and one Mile round

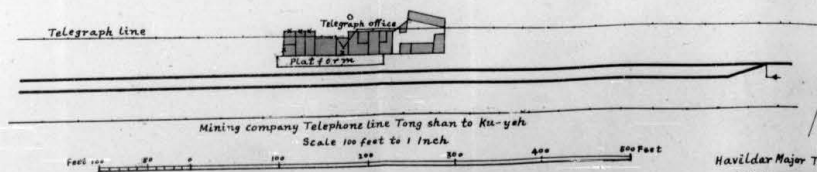
Scale 6 Inches to 1 Mile

Yards 100 200 300 400 500 600 700 800 900 1000

Country flat but not liable to inundation
all streams coloured blue have water
always in them others only after rain



PLAN OF WA-LI STATION



Havildar Major Thakur Singh
SI Burma L.I.

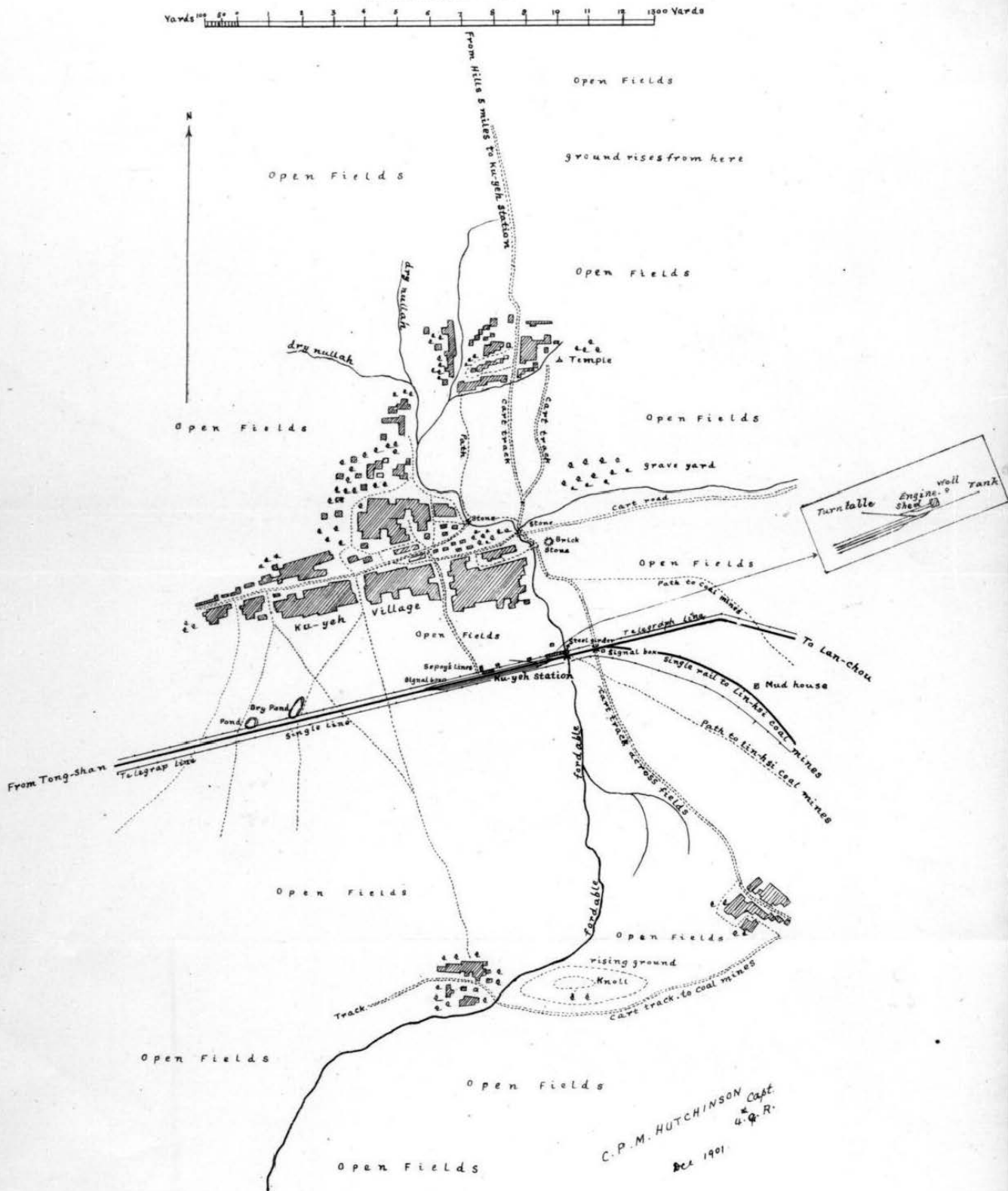
治古

KU-YEH STATION

and one mile round

Scale 6 Inches to 1 Mile

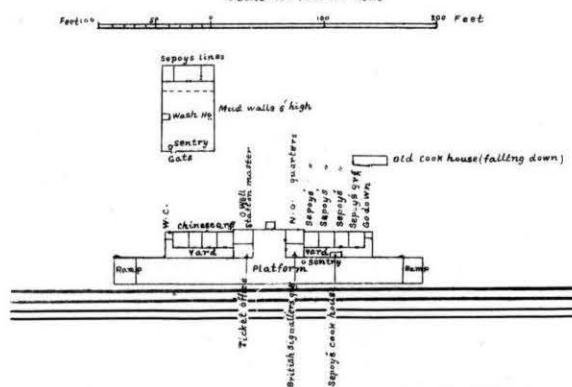
37



古冶

PLAN OF KU-YEH RAILWAY STATION

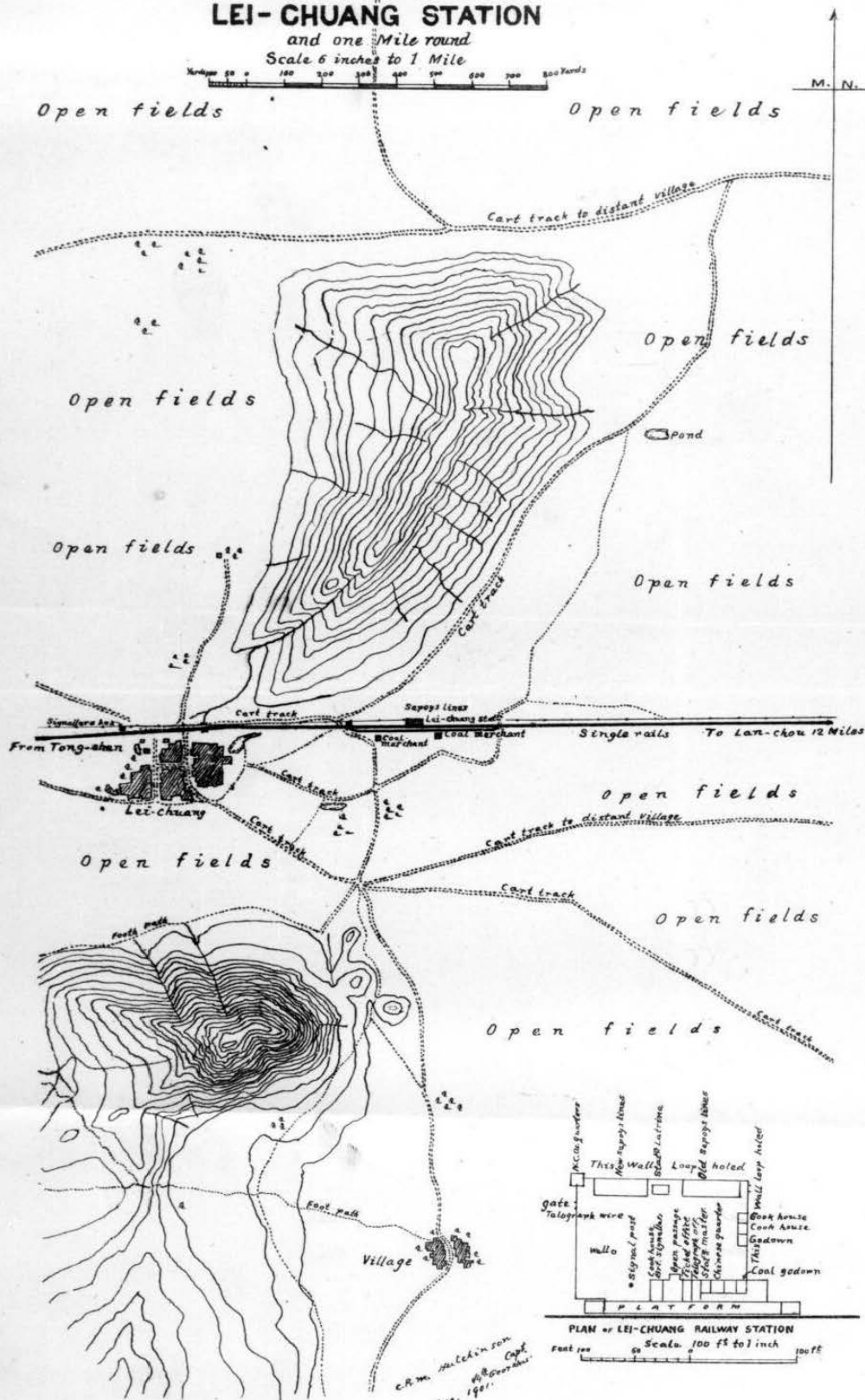
Scale 100 feet to 1 inch



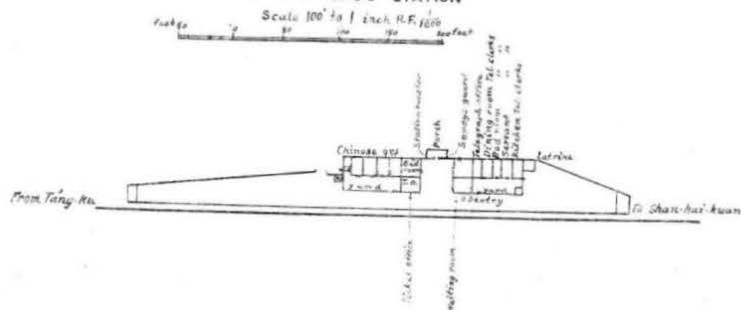
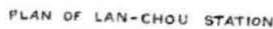
C.R.M. HUTCHINSON
Dec. 1901
Capt.
4th G.R.

and one Mile round
Scale 6 inches to 1 Mile

Scale 6 inches to 1 Mile



and one Mile round
Scale 6 inch to 1 Mile R.F. $\frac{1}{10000}$

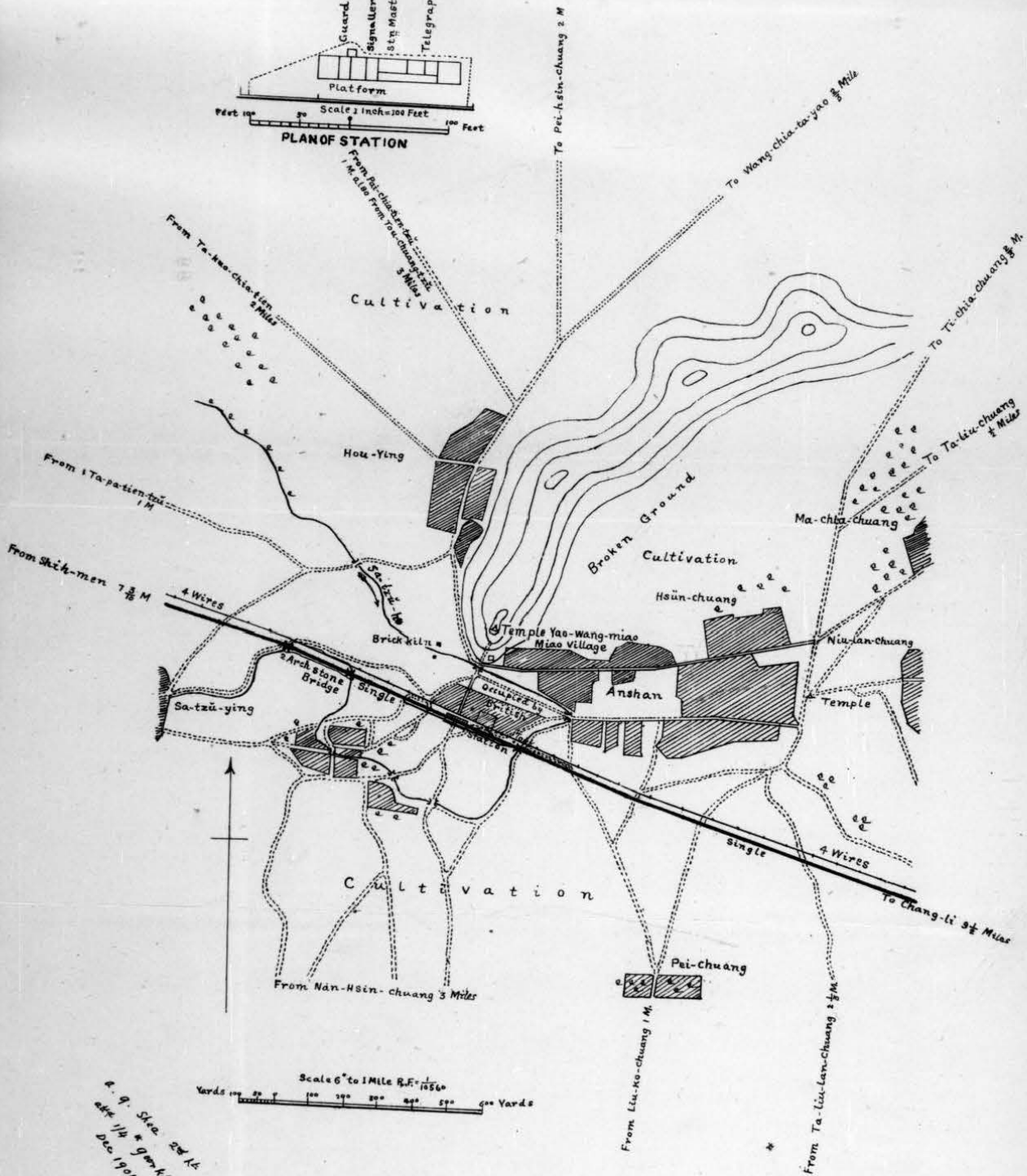
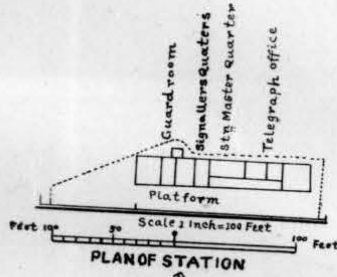


山 安

AN-SHAN POST

42

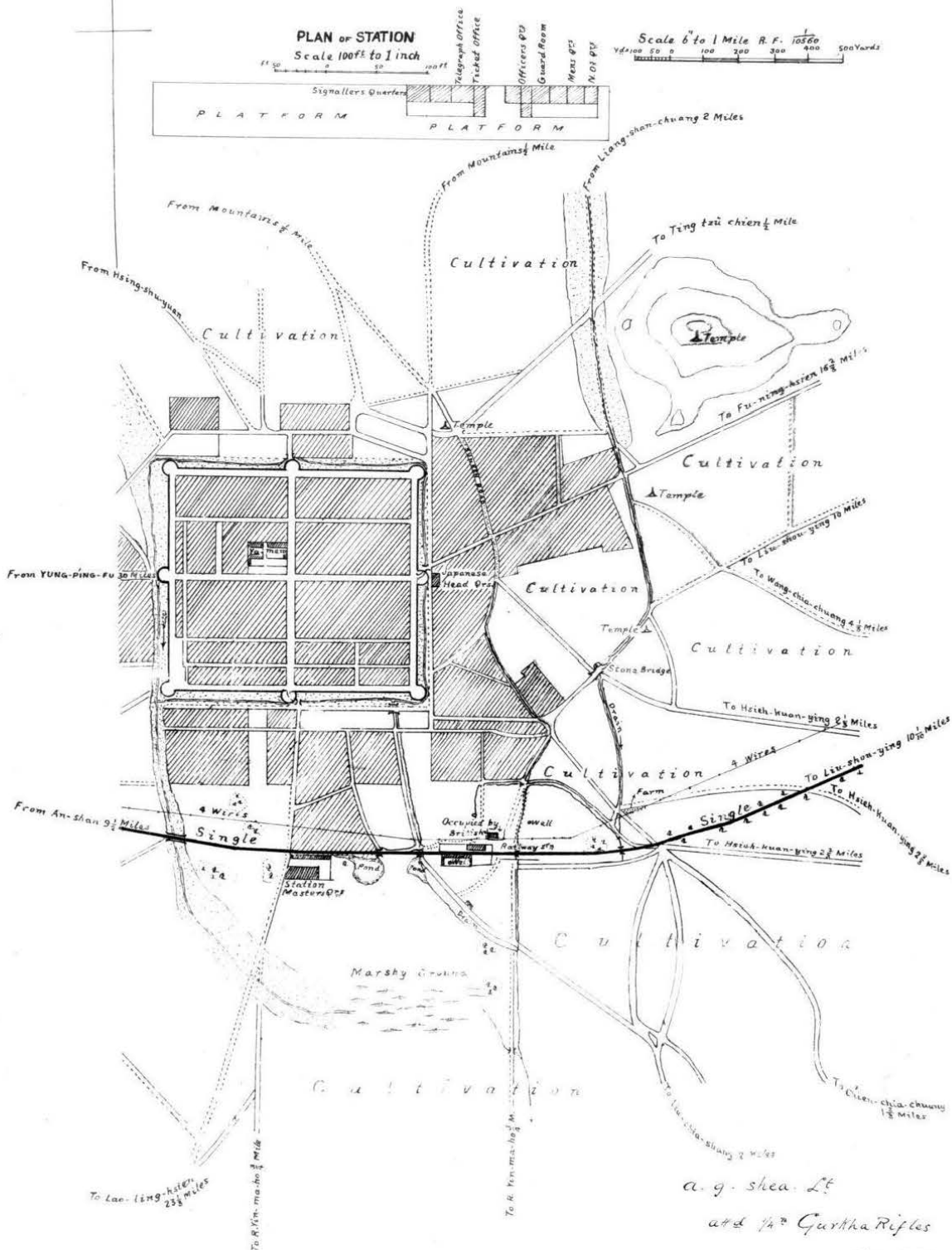
for one mile round



A. G. Shea, 2nd Lt.
 1st Regt. 1st Div. 9th Corps
 Dec. 1901

CHANG-LI RAILWAY STATION
and Country for one Mile round

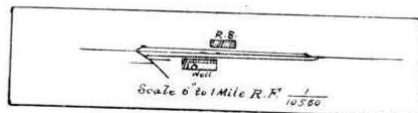
and Country for one Mile round



a. g. shea. L.

at $\frac{1}{4}$ Gurkha Rifles

Dec 1901



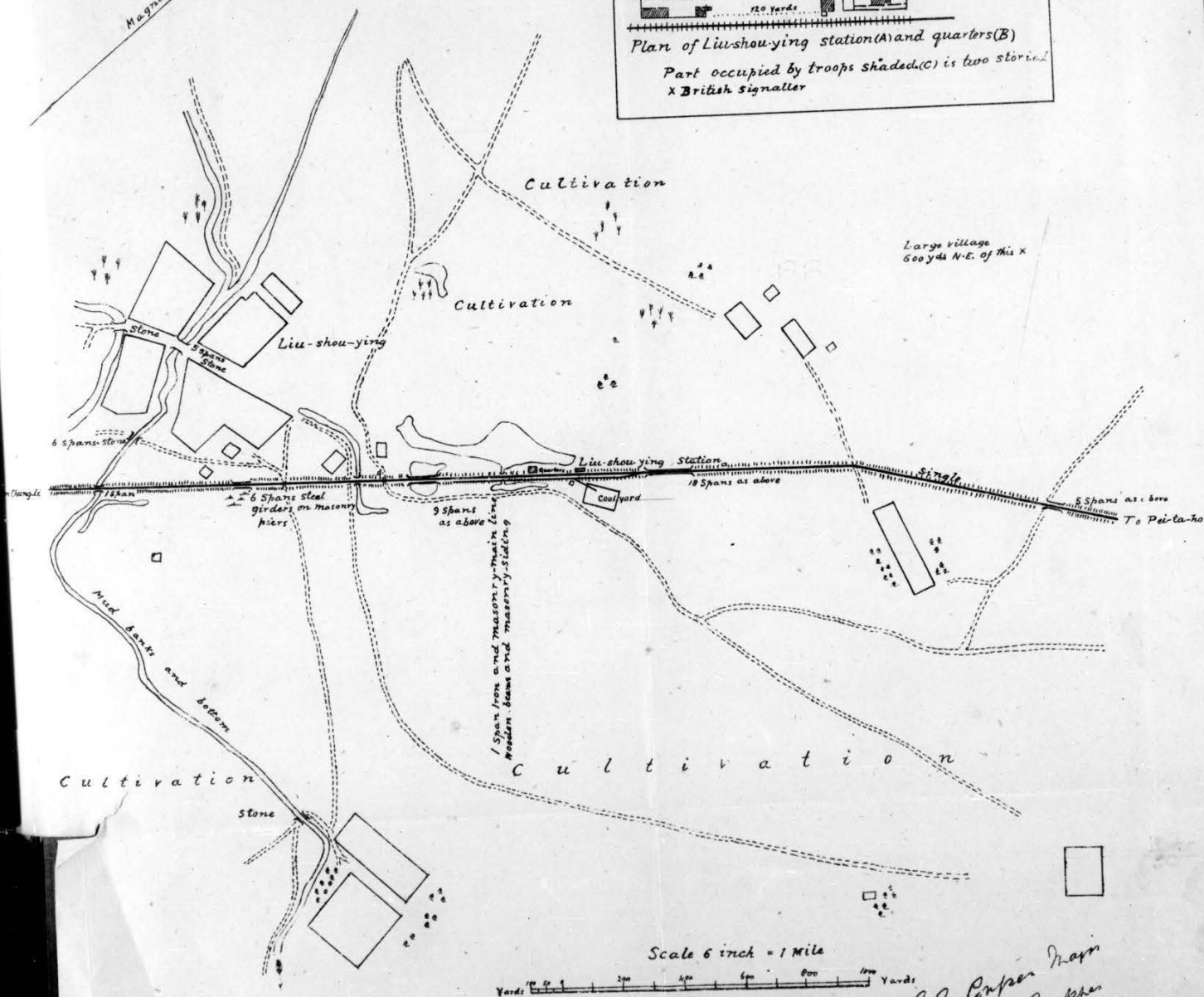
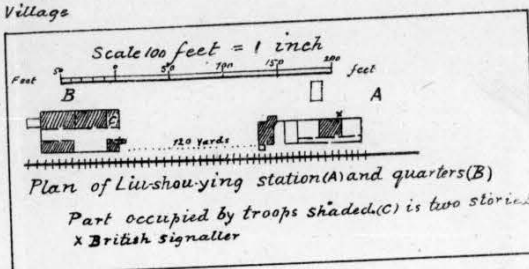
營守留 LIU-SHOU-YING STATION AND one mile round

Village

Village

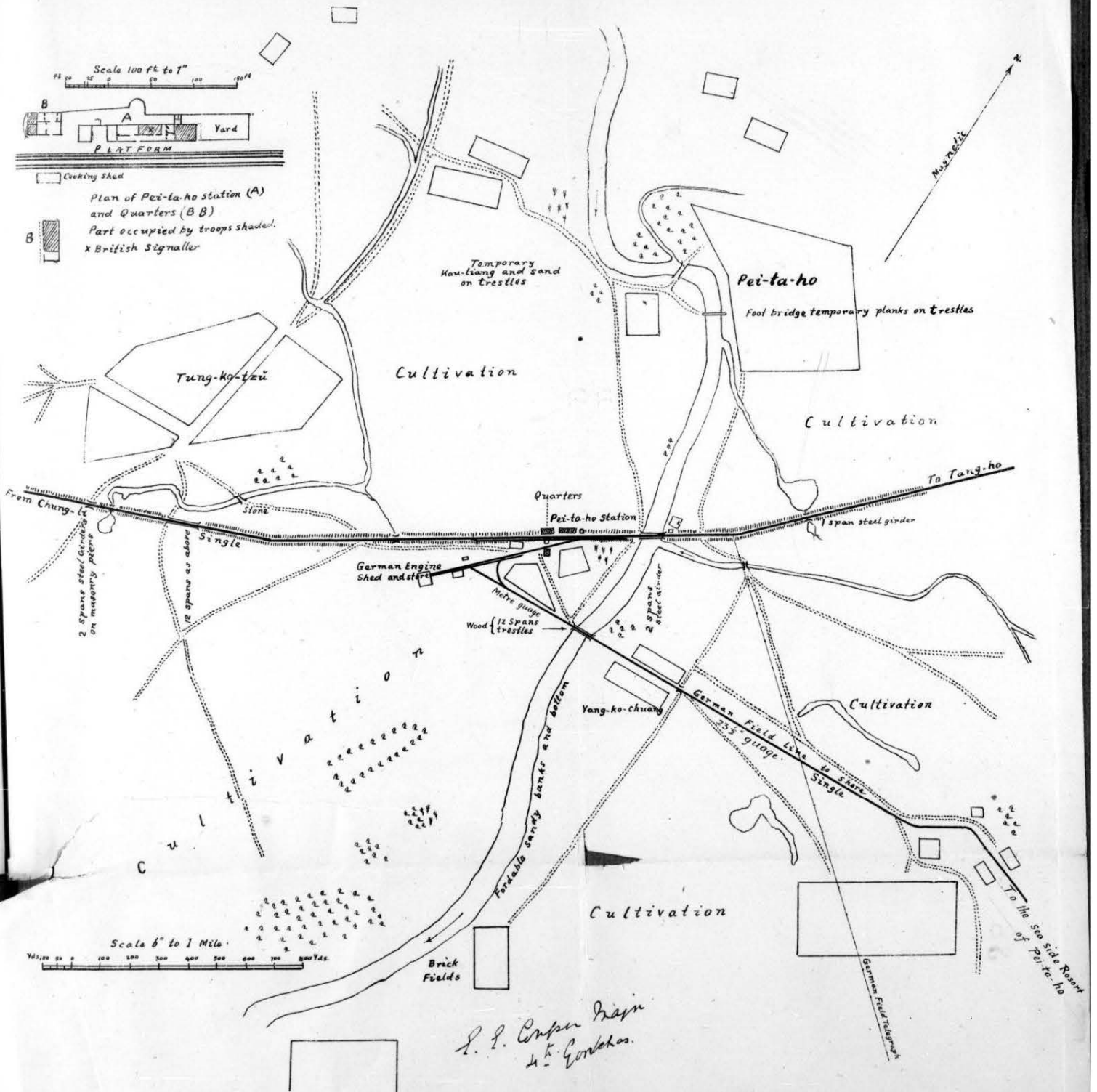
Hills some
miles away

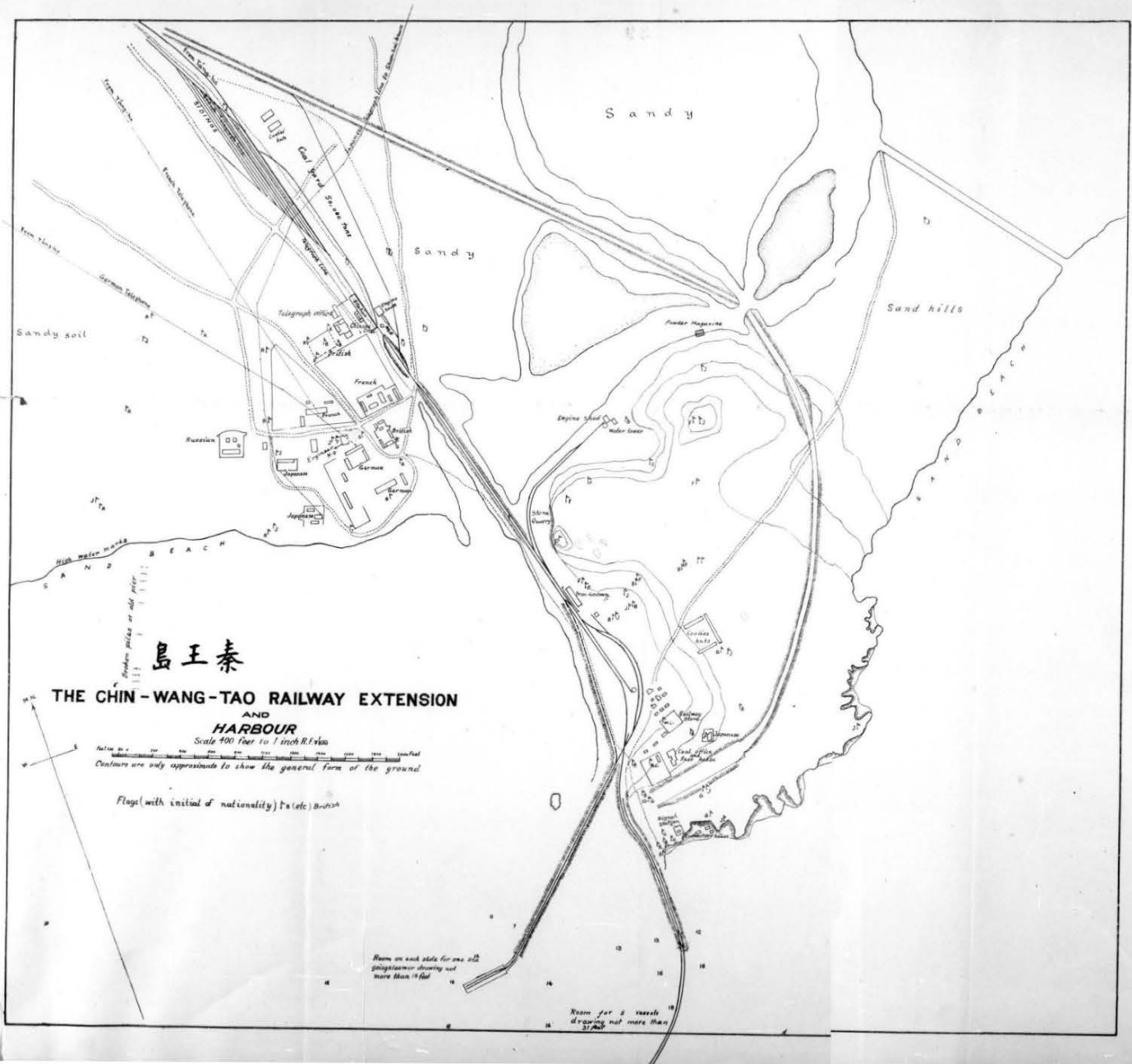
Magnetic

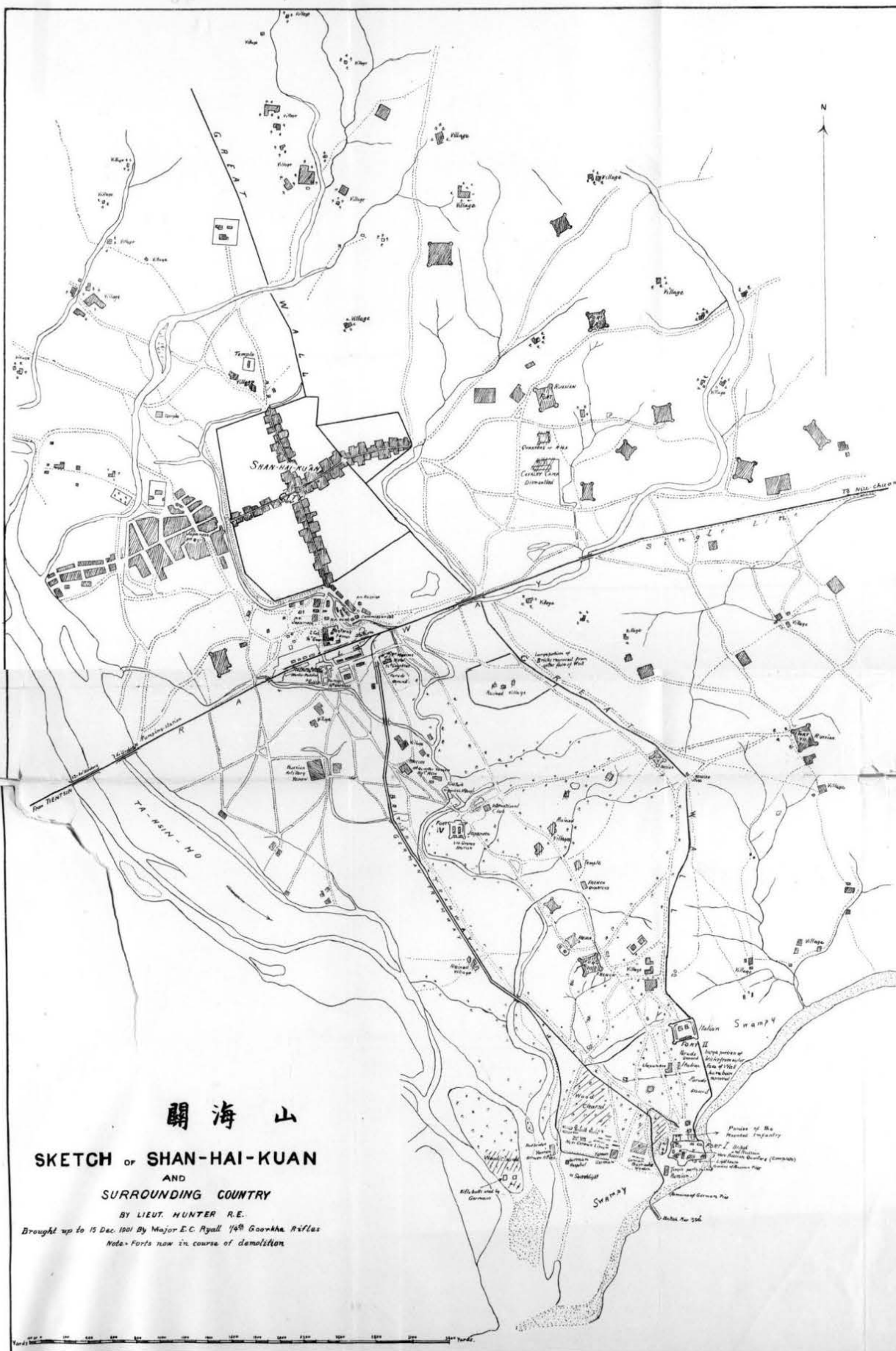


J. E. Cooper Map
H. K. Cooper

PEI-TA-HO RAILWAY STATION and country for one mile round







關海山

PLAN OF RAILWAY PROPERTY AT SHAN-HAI-KUAN

Scale 1 inch to 400 Feet

Feet 0 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63 64 65 66 67 68 69 70 71 72 73 74 75 76 77 78 79 80 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 100 101 102 103 104 105 106 107 108 109 110 111 112 113 114 115 116 117 118 119 120 121 122 123 124 125 126 127 128 129 130 131 132 133 134 135 136 137 138 139 140 141 142 143 144 145 146 147 148 149 150 151 152 153 154 155 156 157 158 159 160 161 162 163 164 165 166 167 168 169 170 171 172 173 174 175 176 177 178 179 180 181 182 183 184 185 186 187 188 189 190 191 192 193 194 195 196 197 198 199 200 201 202 203 204 205 206 207 208 209 210 211 212 213 214 215 216 217 218 219 220 221 222 223 224 225 226 227 228 229 230 231 232 233 234 235 236 237 238 239 240 241 242 243 244 245 246 247 248 249 250 251 252 253 254 255 256 257 258 259 260 261 262 263 264 265 266 267 268 269 270 271 272 273 274 275 276 277 278 279 280 281 282 283 284 285 286 287 288 289 290 291 292 293 294 295 296 297 298 299 300 301 302 303 304 305 306 307 308 309 310 311 312 313 314 315 316 317 318 319 320 321 322 323 324 325 326 327 328 329 330 331 332 333 334 335 336 337 338 339 340 341 342 343 344 345 346 347 348 349 350 351 352 353 354 355 356 357 358 359 360 361 362 363 364 365 366 367 368 369 370 371 372 373 374 375 376 377 378 379 380 381 382 383 384 385 386 387 388 389 390 391 392 393 394 395 396 397 398 399 400 401 402 403 404 405 406 407 408 409 410 411 412 413 414 415 416 417 418 419 420 421 422 423 424 425 426 427 428 429 430 431 432 433 434 435 436 437 438 439 440 441 442 443 444 445 446 447 448 449 450 451 452 453 454 455 456 457 458 459 460 461 462 463 464 465 466 467 468 469 470 471 472 473 474 475 476 477 478 479 480 481 482 483 484 485 486 487 488 489 490 491 492 493 494 495 496 497 498 499 500 501 502 503 504 505 506 507 508 509 510 511 512 513 514 515 516 517 518 519 520 521 522 523 524 525 526 527 528 529 530 531 532 533 534 535 536 537 538 539 540 541 542 543 544 545 546 547 548 549 550 551 552 553 554 555 556 557 558 559 560 561 562 563 564 565 566 567 568 569 570 571 572 573 574 575 576 577 578 579 580 581 582 583 584 585 586 587 588 589 590 591 592 593 594 595 596 597 598 599 600 601 602 603 604 605 606 607 608 609 610 611 612 613 614 615 616 617 618 619 620 621 622 623 624 625 626 627 628 629 630 631 632 633 634 635 636 637 638 639 640 641 642 643 644 645 646 647 648 649 650 651 652 653 654 655 656 657 658 659 660 661 662 663 664 665 666 667 668 669 670 671 672 673 674 675 676 677 678 679 680 681 682 683 684 685 686 687 688 689 690 691 692 693 694 695 696 697 698 699 700 701 702 703 704 705 706 707 708 709 710 711 712 713 714 715 716 717 718 719 720 721 722 723 724 725 726 727 728 729 730 731 732 733 734 735 736 737 738 739 740 741 742 743 744 745 746 747 748 749 750 751 752 753 754 755 756 757 758 759 760 761 762 763 764 765 766 767 768 769 770 771 772 773 774 775 776 777 778 779 780 781 782 783 784 785 786 787 788 789 790 791 792 793 794 795 796 797 798 799 800 801 802 803 804 805 806 807 808 809 810 811 812 813 814 815 816 817 818 819 820 821 822 823 824 825 826 827 828 829 830 831 832 833 834 835 836 837 838 839 840 841 842 843 844 845 846 847 848 849 850 851 852 853 854 855 856 857 858 859 860 861 862 863 864 865 866 867 868 869 870 871 872 873 874 875 876 877 878 879 880 881 882 883 884 885 886 887 888 889 890 891 892 893 894 895 896 897 898 899 900 901 902 903 904 905 906 907 908 909 910 911 912 913 914 915 916 917 918 919 920 921 922 923 924 925 926 927 928 929 930 931 932 933 934 935 936 937 938 939 940 941 942 943 944 945 946 947 948 949 950 951 952 953 954 955 956 957 958 959 960 961 962 963 964 965 966 967 968 969 970 971 972 973 974 975 976 977 978 979 980 981 982 983 984 985 986 987 988 989 990 991 992 993 994 995 996 997 998 999 1000

A = Locomotive yard; Aard
B = B. S. Co. Quarters
From B. R. A. plans

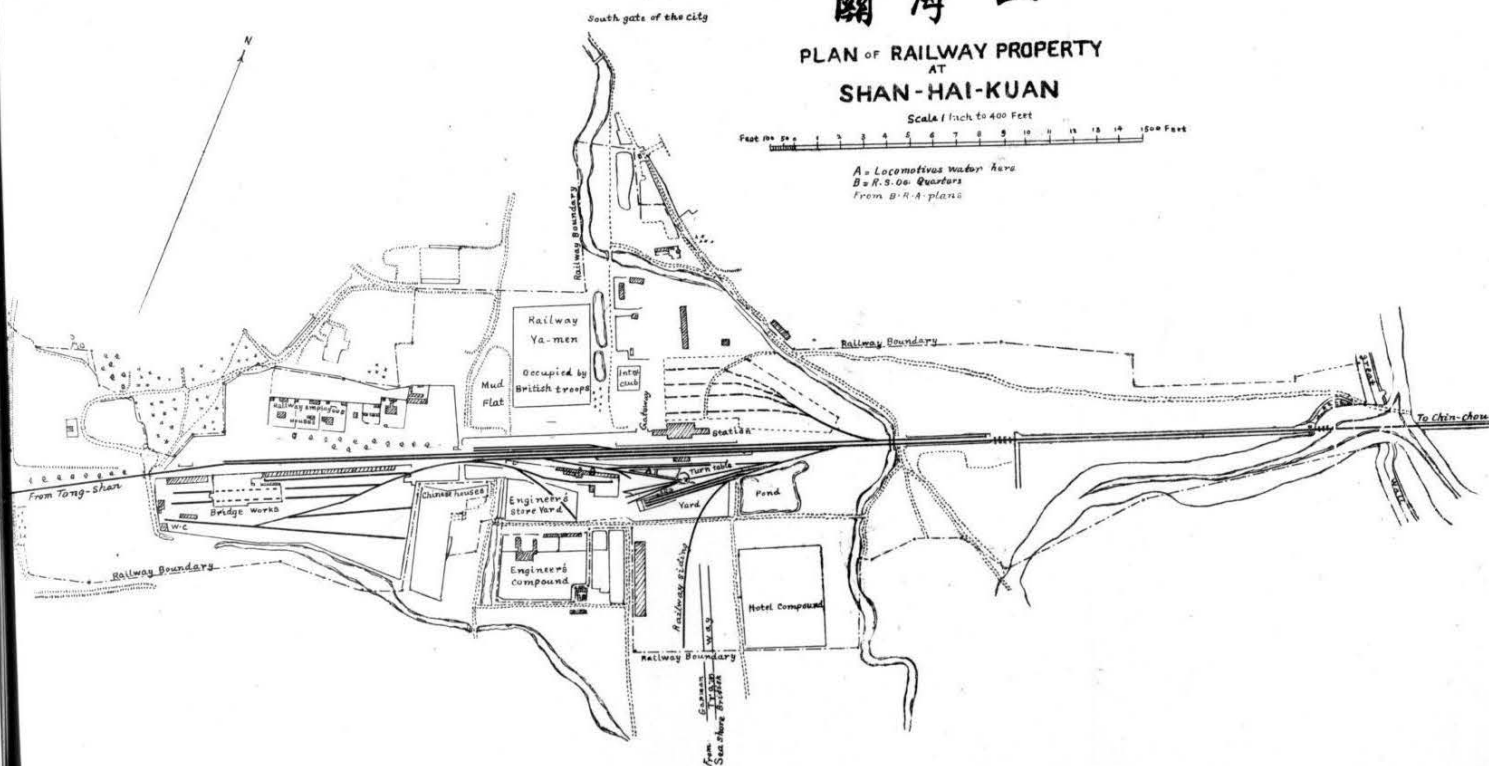


Fig 1
SECTION

Deck bridge
12 feet span, plate girder
girders 15 feet over all

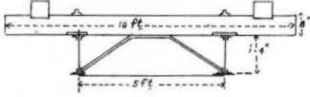


Fig 2
SECTION

Deck bridge
20 feet span, plate girders
girders 23 feet over all

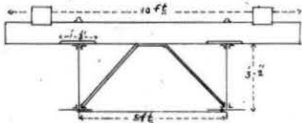


Fig 3
SECTION

Deck bridge
30 feet span, plate girders.
girders 34 feet over all

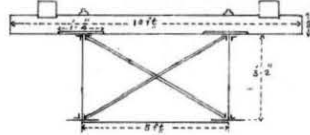


Fig 4
SECTION

Deck bridge
60 feet span, plate girders
girders 65 feet over all

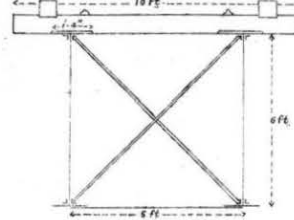
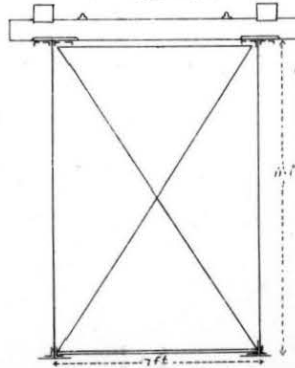


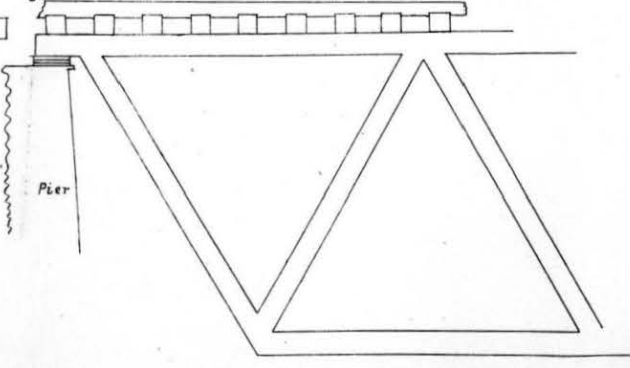
Fig 5

Deck bridge
100 feet span, lattice girders
girders 106 feet over all

SECTION



side elevation



Diagrams showing types of bridges (deck & through & through)
on North China Railway Peking to Shan-hai-Kuan

Fig. 1
60 feet span & deck bridge double line
SECTION

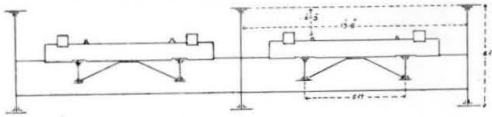


Fig. 3
200 feet span through bridge single line
SECTION

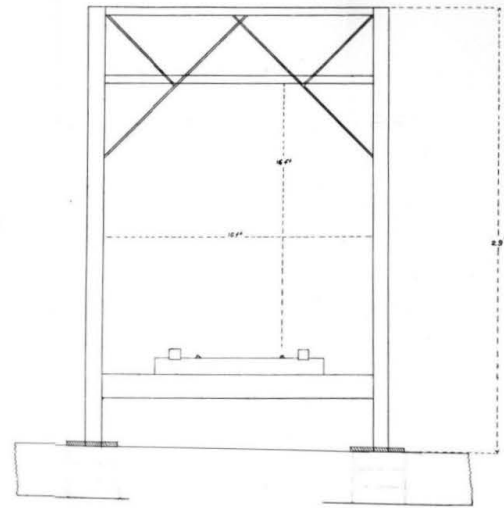
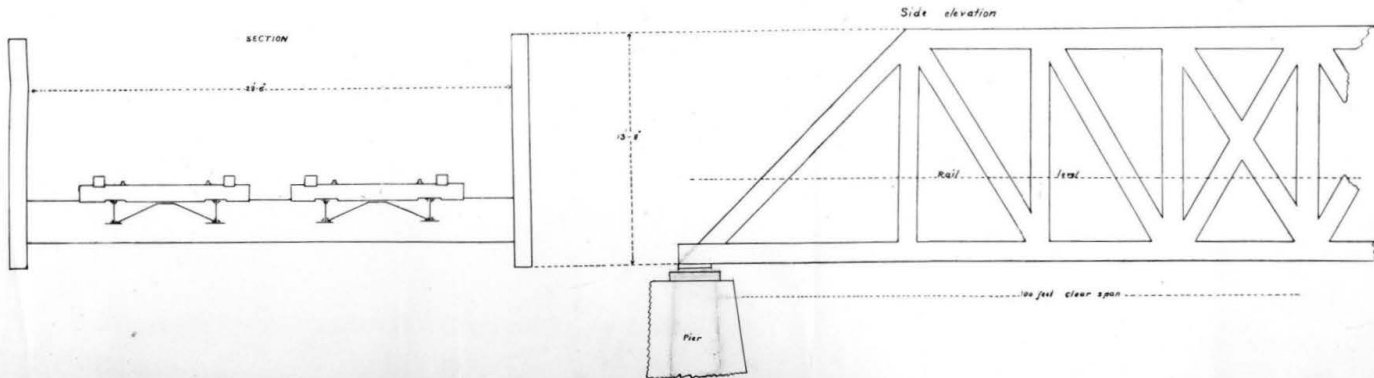


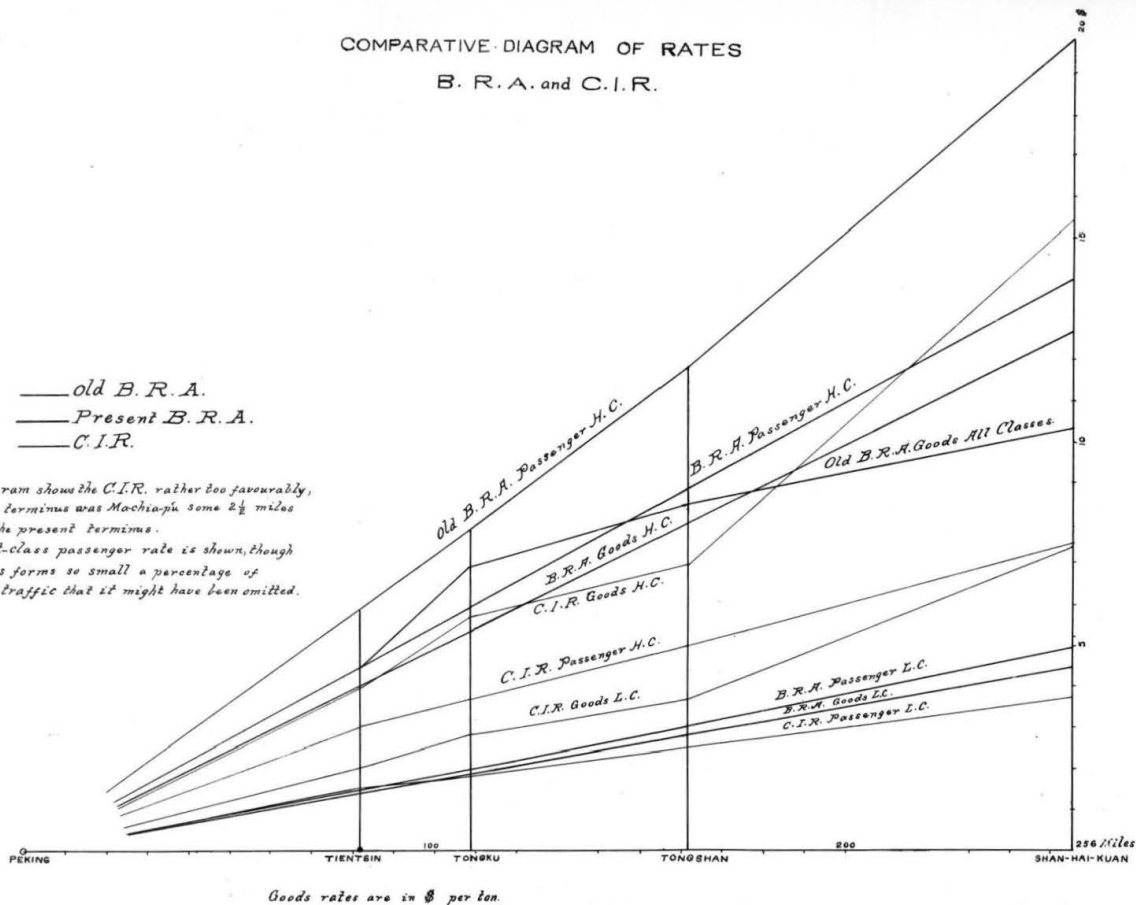
Fig. 2
100 feet span & through bridge double line



COMPARATIVE DIAGRAM OF RATES
B. R. A. and C. I. R.

— old B. R. A.
— Present B. R. A.
— C. I. R.

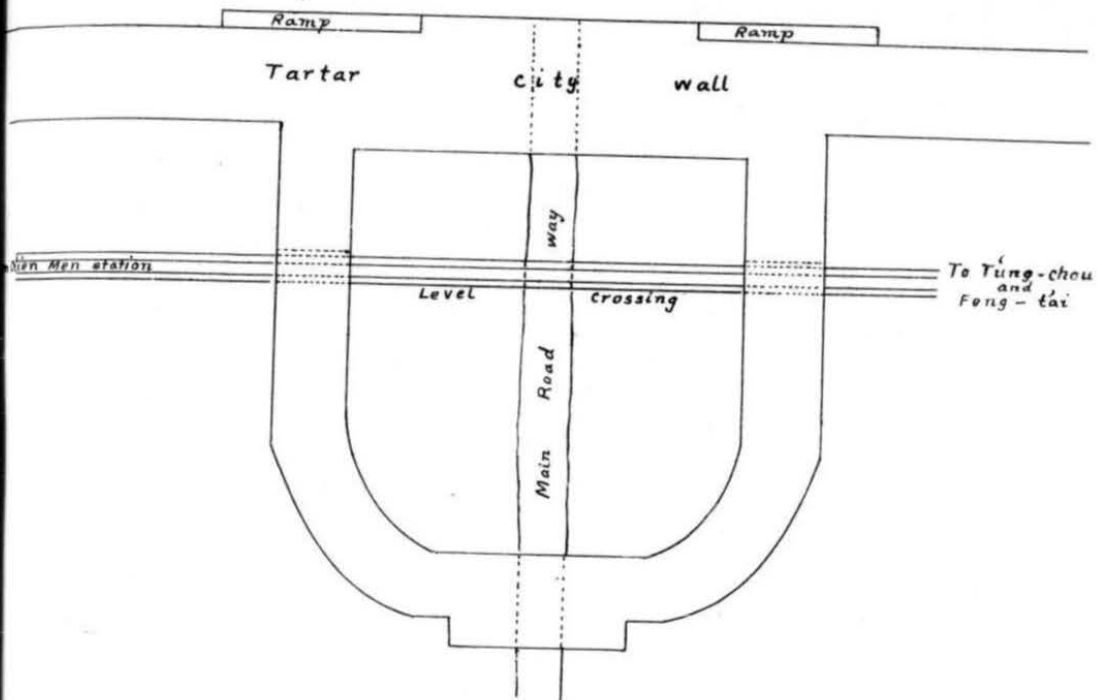
This diagram shows the C. I. R. rather too favourably, as the old terminus was Machiapu some $2\frac{1}{2}$ miles short of the present terminus.
The first-class passenger rate is shown, though that class forms so small a percentage of the total traffic that it might have been omitted.



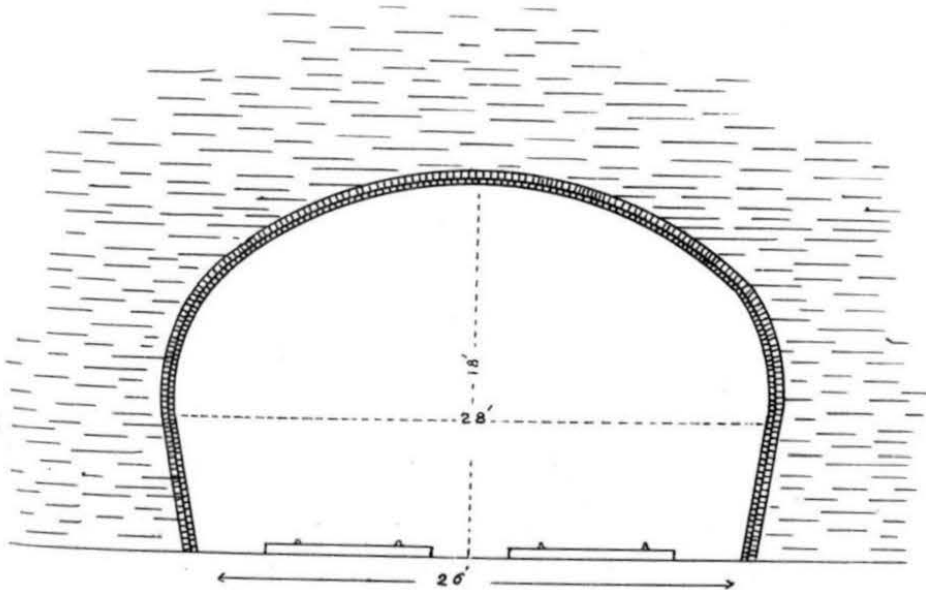
Goods rates are in \$ per ton.

85. 1000 1000
13 4
02

Tunnel through The
Hata Men Bastion
PLAN
Scale 120' = 1"



Front Elevation
Scale 8' = 1"



- (1) T'ANG-KU Station. Platform looking North.
- (2) T'ANG-KU Wharf.
- (3) HSIN-HO Wharf and Crane.
- (4) HSIN-HO Wharf and Barracks.
- (5) TIEN-TSIN UP Platform looking North showing overhead bridge.
- (6) TIEN-TSIN Station looking South showing Engine shed and Water tower.
- (7) The HAI-HO at TIEN-TSIN taken from the road between the French Post bridge and the Station.
- (8) YANG-TS'UN railway bridge from North end.
- (9) Floods near YANG-TS'UN. (August 1901).
- (10) LO-FA Station.
- (11) LONG-FANG Station.
- (12) SH-I'ING Station.
- (13) HUANG-TS'UN Station.
- (14) FANG-T'AI showing some railway material collected after Boxer troubles 1900.
- (15) Ruins of old Station MA-CHIA-S'U destroyed by Boxers 1900.
- (16) Gate in the Chinese City wall by which the line to the TEMPLE of HEAVEN enters the Chinese City PEKING, taken from the outside showing bridge over road.
- (17) Gate from the inside showing Engine watering tank on the City wall.
- (18) Panoramic view of the YUNG-TING-MEN (TEMPLE of HEAVEN) Station.
- (19) The CH'IAO-MEN LA CHIER, or main North and South road PEKING looking North taken from top of South Gate Chinese City.
- (20) CH'IAO-MEN Station and Post.

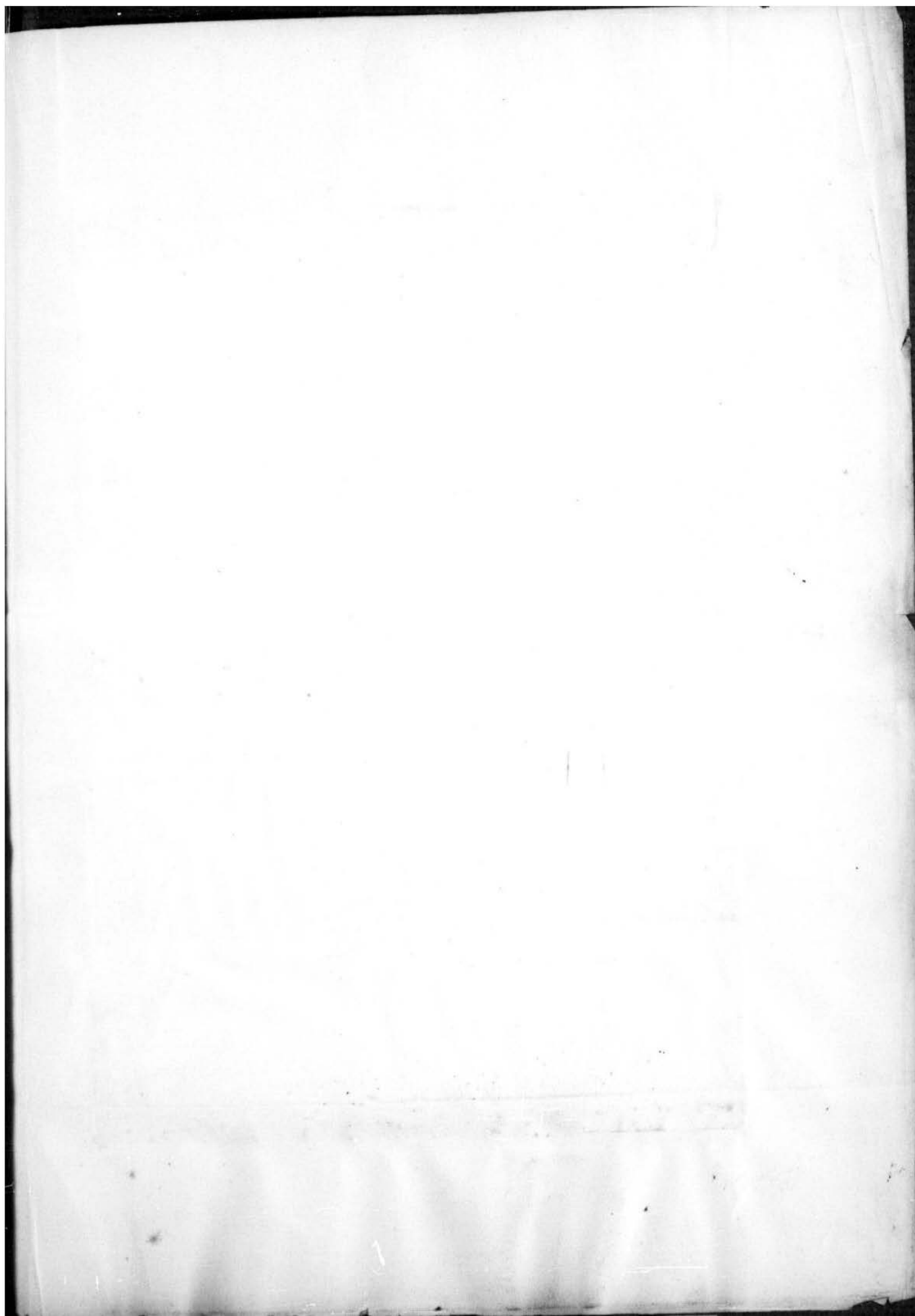
LIST OF PHOTOGRAPHS T'ANG-KU TO SHAN-HAI-KUAN SECTION.

- (21) LU-T'AI FORT.
- (22) View of country round LU-T'AI, showing demolished Fort.
- (23) TONG-SHAN Station.
- (24) Cantonese Barracks TONG-SHAN.
- (25) Officers Quarters TONG-SHAN.
- (26) Hospital and new workshops TONG-SHAN.
- (27) MAI-CHOU Post.
- (28) SHIH-WEI Barracks.
- (29) SHAN-HAI-KUAN Station.
- (30) North view of railway Yamen SHAN-HAI-KUAN.
- (31) English and German railway lines at SHAN-HAI-KUAN.
- (32) Railway Hotel SHAN-HAI-KUAN, occupied by Gurkhas.

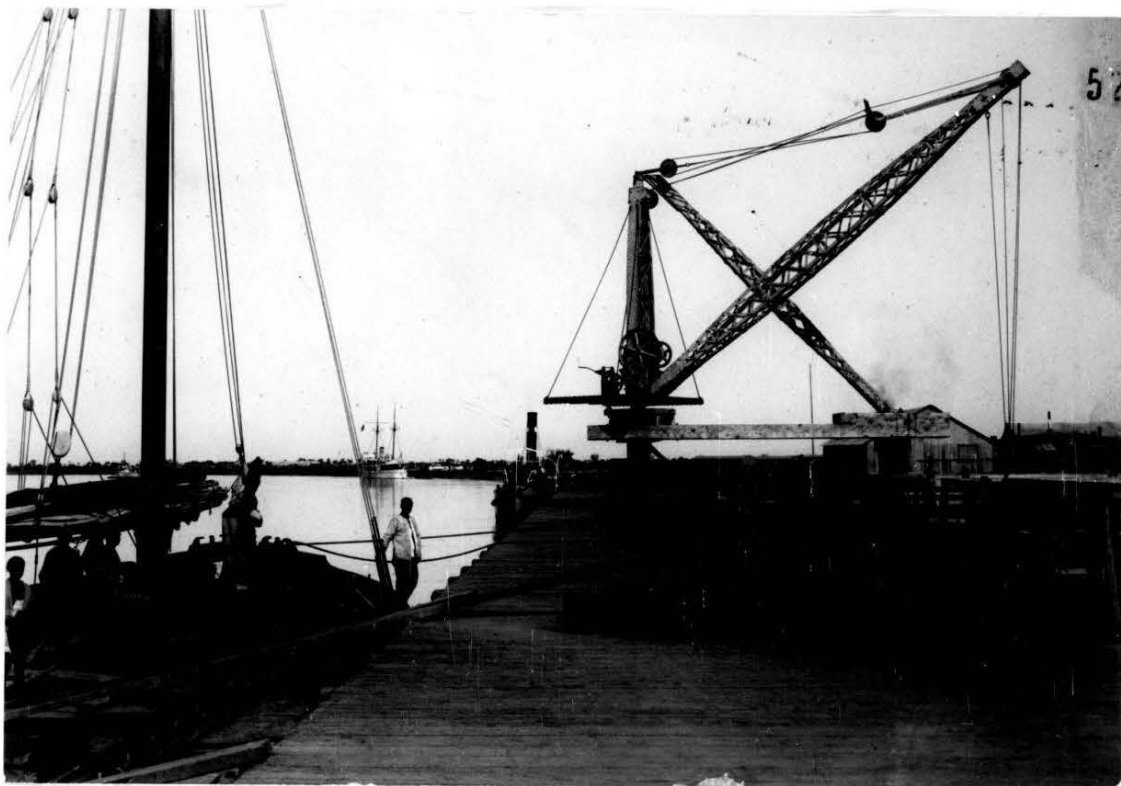


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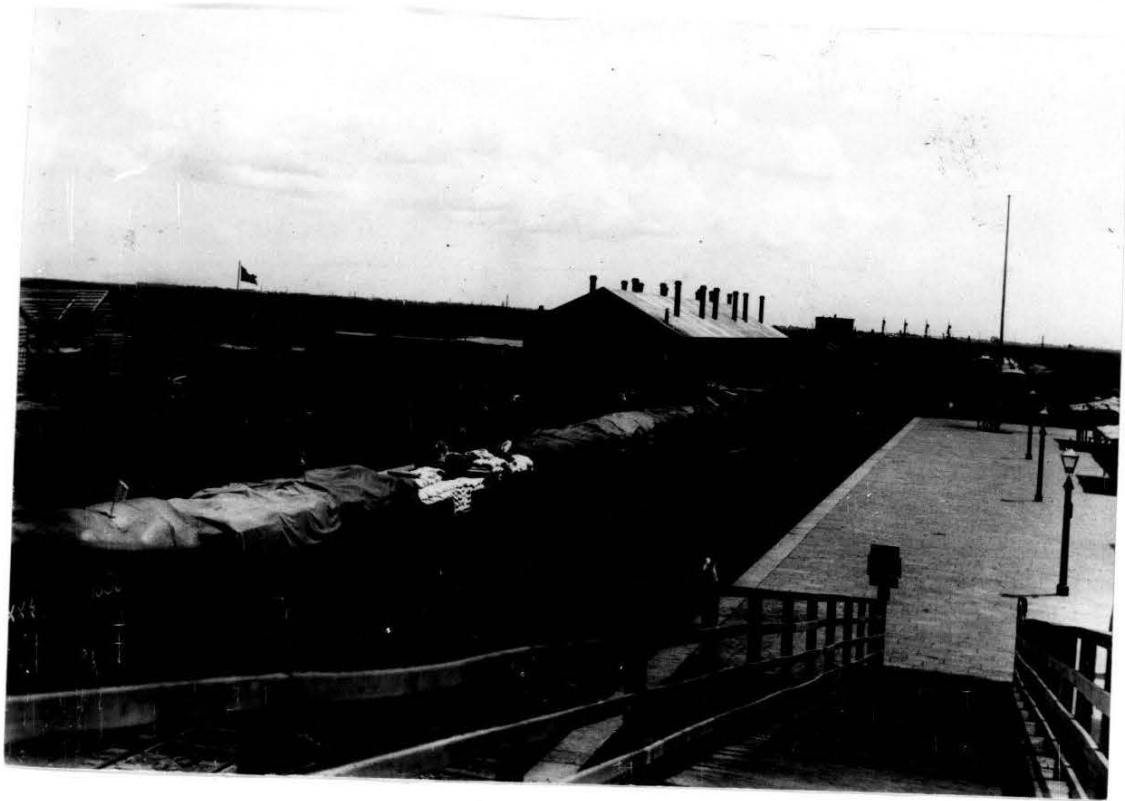


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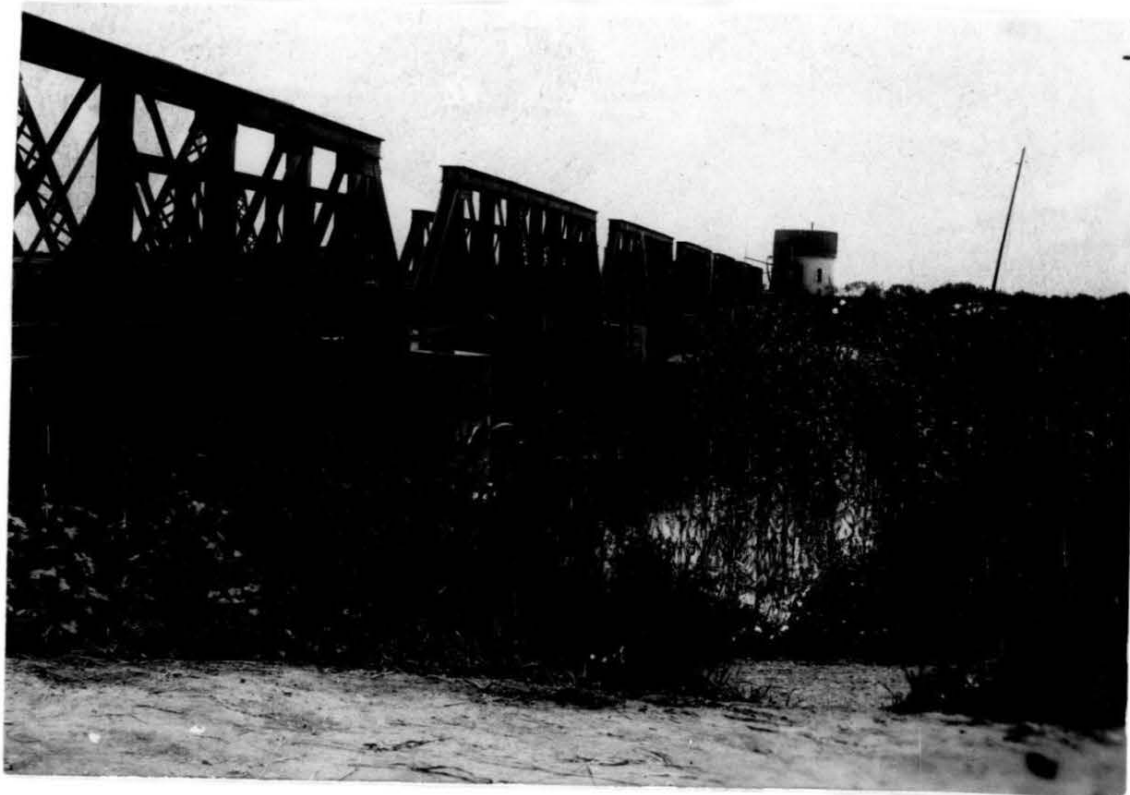




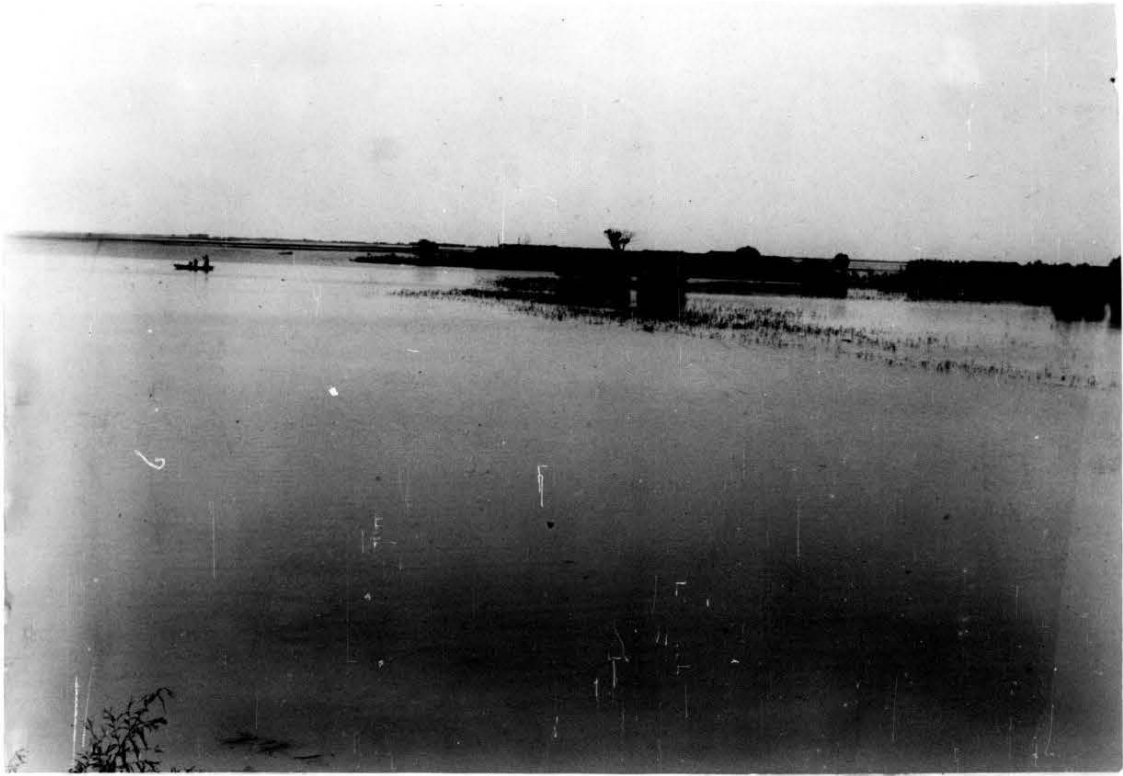
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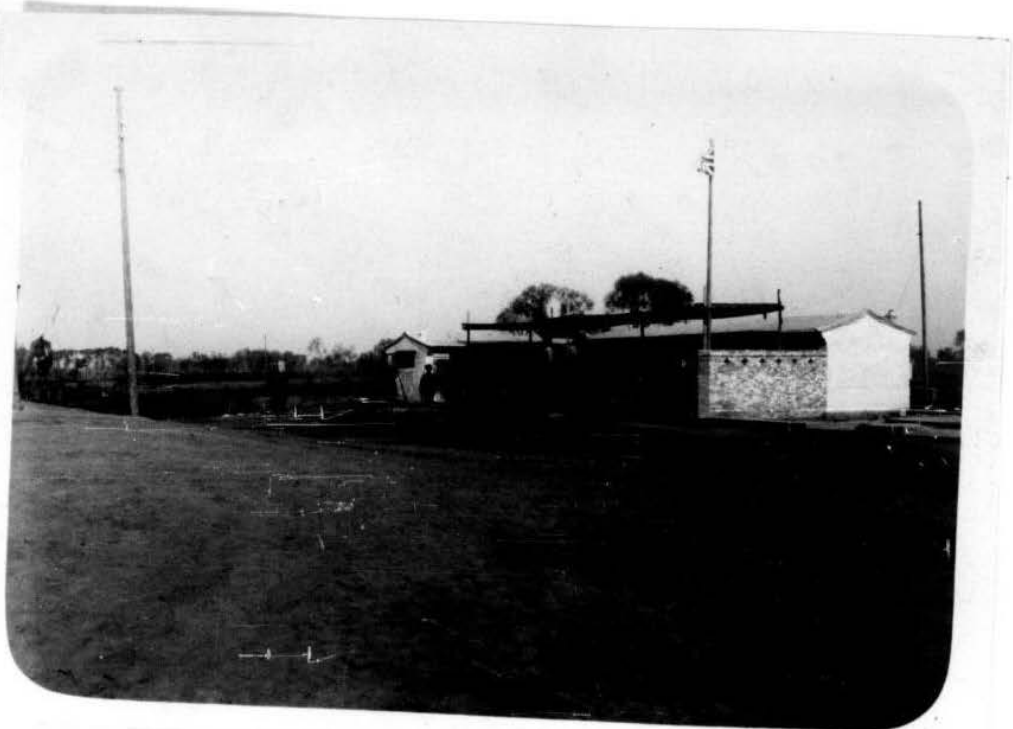
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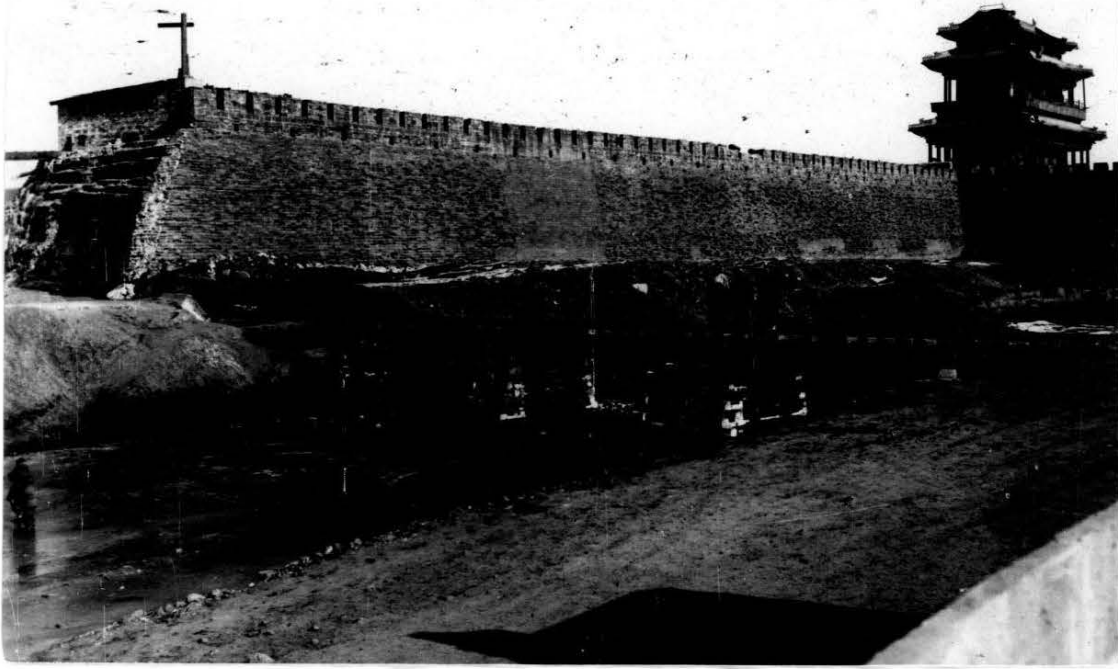
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Peking.

26 January, 1903.

No 38.

My Lord,

In the course of a recent correspondence with Mr Kinder, as Managing Engineer of the Imperial Northern Railways, upon the subject of a claim submitted by Messrs. Whittall and Co. for bridge material, I quoted a statement made by Sir E. Satow in a letter dated the 22nd September last to Mr Hillier, then acting as British Delegate on the Bankers' Commission at Shanghai, to the effect that certain charges should come out of the sum of £500,000 in Chinese 4% Bonds reserved to meet the Northern Railways claim.

This statement produced a letter from Mr E. Cousins, as Representative of the British and Chinese Corporation, copy of which I have the honour to enclose, stating that this was the first intimation that the bondholders have had

The Marquess of Lansdowne K.G.

etc. etc. etc.

Mr Cousins.
10 Jan. 1903.

Mr Cousins.
26 Jan. 1903.

2 104/ 28

had of the manner in which it is proposed to liquidate their claim, and asking that, pending the receipt of a more official communication, action in the ~~sense~~ indicated may be suspended.

In my reply to Mr Cousins, copy of which I have also the honour to enclose, I have informed him that the manner of liquidation would not appear to have been as yet finally decided upon, and I have explained why the claim was included in the British Government claim, as put before Your Lordship in Sir E. Satow's despatch No 172 of the 16th June last, whilst pointing out that it cannot be determined until the accounts come to hand what the actual amount of money due to the shareholders will be.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Townley

COPY.

Tientsin.

10 Jan. 1903.

Sir,

In your letter of 8th Dec. to Mr Kinder occur the words :-
" the sum of £500,000 in Chinese bonds reserved to meet the Northern Rys. Claim".

This appears to be the first intimation the bondholders have had as to the medium in which it is intended to liquidate the claim.

Pending consultation with my principals and communication in more official manner and in fuller detail, I beg permission to call your attention to the fact that if the Railways are relegated to the 4% 39 year Bonds for Capital with which to restore the ante-Boxer condition of the line, as well as to meet the matured obligations, and are obliged to receive these bonds at par, the line will fall short of such restoration for some time to come.

I earnestly beg therefore, on behalf of the interests I represent, that action may be suspended, particularly in respect of any engagements with the Chinese Govt. as to a disposition of a possible balance of Reserve Fund.

Walter Townley Esq.

etc. etc. etc.

I have etc. (Sd.) Ed. Cousins.

Representing the B. & C. Corp'n.

Probably the balance of the
£500,000

3 1571 28

26th Jan. 1903

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I beg to acknowledge the receipt of your letter of the 10th inst. upon the subject of the Northern Railways Indemnity claim, and a statement made in a letter to Mr. Kinder by me, to the effect that the sum of £500,000 intended to meet the claim had been reserved in Chinese 4 % bonds. The above statement was taken from a letter written to Mr Hillier, as British Delegate on the Bankers' Commission at Shanghai by Sir E. Satow, on the 22nd September last, respecting Messrs. J. Whittall & Co's claim.

It would not appear that any final decision has as yet been arrived at as to the manner in which the Northern Railways indemnity claim should be dealt with, nor indeed as to the actual amount of money that will be required to liquidate it.

The claim was included in the British Government claim because His Majesty's Government had advanced £90,000 on the indemnity or compensation to be received by

Edmund Cousins Esq,
Tientsin.

the

34-1871 28

the British and Chinese Corporation. In addition to the above mentioned sum, it would seem that £30,000 were expended on the railway during its administration by the British military authorities, presumably out of revenue, and it has been estimated that another £115,000 will be required to put the railway in complete order. These figures can of course only be verified when the accounts of the British Railway Administration come ~~in~~^{to} hand, and the present state of the line can be accurately gauged.

I am transmitting a copy of your letter under reply to Lord Lansdowne, and will let you know in due course what answer I receive from His Lordship.

I am,

Sir,

Your obedient Servant,

5 1871 23

Peking.

12 March. 1903.

No. 98.
Confidential.

My Lord,

I have the honour to report that the Imperial Northern Railways are constructing a branch line from a point near Fengtai, on the Peking-Tientsin line, which runs due North, crossing the Luhan line by an elevated bridge, passes close to the Hsi Chih Men, or North West Gate of the Tartar City, and then continues north for a mile or more, with a branch to the Summer Palace. The line will be open for general traffic, whilst it is hoped to eventually extend

The Marquess of Lansdowne K. G.

etc. etc. etc.

20 - 1912 Rd 4

extend it to Kalgan; it will also probably form a link in the circular line round Peking which Mr. Kinder hopes to construct some day. It would in this case connect with the branch line to Tungchow which was constructed under the direction of the B. Railway Administration.

The French and Belgian Ministers made strenuous efforts to secure the construction of this line, and of the newly laid line from a point on the Luhan line to the Western Tombs, for the Franco-Belgian enterprise, and would probably have secured the building of both lines but
for

87
for Viceroy Guan Shih-k'ai's firm
opposition.

I have the honour to be,
With the highest respect,
My Lord,
Your Lordship's most obedient
humble servant.
Walter Townley

PEKING.

9 April, 1903.

No. 138.

My Lord,

I have the honour to transmit herewith to Your

Lordship, in original, and with reference to your Despatch Number 42 of the 13 February last, a detailed statement of the Northern Railways claim as furnished to me by Mr. Cousins.

Your Lordship will perceive that the total claim for both intra and extra mural lines amounts to £448,836-13-9 to which will have eventually to be added a sum of £4000, the estimated cost of the resurvey of the line which will have to be made in order to establish the titles to lands of which the deeds were lost in the destruction of the head office at Tientsin. The above mentioned sum, divided I

am

THE

MARQUESS OF LANSDOWNE, K.G.

&c.

&c.

&c.

2 1961 23

1903.

March 12.

Peiing

to Tientsin

to 98

Capital.

Wed. 27 April 1903 -

by 603.

Northern Railways.

Project of branch to Summer

Place, with possible extension

to Kalgan.

Prin. (Chin.)

This looks like a severe

for Mr. Cousins, but he may

spec. considerable

opinion from Mr. Cousins before

he sends his line to

Kalgan.

27 April 1903.

Correspondence

347. C. L.

Summary of Claim
to Cousins
4 Apr. 1903

Correspondence (Conf.) of

Contents of form

for project. 42.

Mr. Cousins will be

proposed to arrange

to build a Peking-

Kalgan line, which

will be through down-

to Jiaotai to

Peiping.

J.H.C.

am told, as well as may be between the two lines, is made up of:-

Intra-mural line	£319,680 .18. 10
Extra-mural line	£121,655. 14. 11
Contingent fund	£ 7,500. 0. 0
	<u>£448,835. 13. 9</u>

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his Despatch Number 320 of the 29 October last, has caused him to place the engineering department's claim for damage to that line at £104,010. Only a small portion of this last figure covers damage to, rolling stock, and that chiefly applicable to engines, since it was not possible to make a full separate allocation of rolling stock to the one line or the other, and where separation has not been made the rolling stock and supplies have been attributed to the intra-mural line.

Mr.

Mr Cousins tells me that it is possible that the sum of £300,000 claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which, he is given to understand, amounts to about £150,000, although as far as he can ascertain no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to Your Lordship at a later date.

From the appended statement, it would however appear that the sum of £500,000 claimed on account of the intra-mural line should prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its

its rolling stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which however in the shape of bonds is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to Your Lordship upon its receipt.

Mr Cousins points out that the sum of £30,000 referred to in Your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which Your Lordship will see that the sum of £90,000 advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr Cousins and Mr Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of £7,500 seems to be a necessary precautionary measure to take, and if the money is not required it will not be used.

References

References 15. 16. and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although in most cases the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon £9000 at which the amount of possible compensation has been fixed may be somewhat reduced. A notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the notice. It is clear that without such public announcement the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement

3-1961 Z³

statement of claims if the document is printed, as press of work has prevented a copy being made here.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Foxley

P.S. Since writing the above I have received an amended statement from Mr Cousins which includes the £4000 on account of resurvey, together with a memorandum explaining the necessity for the same. In transmitting these documents to Your Lordship I have also the honour to enclose copy of a letter just received from Mr Cousins covering the Railway claim, pointing out that the Railway Administration is seriously hampered by want of funds, and expressing a hope that His Majesty's Government will come to an early decision as to when and how the final claim allowed will be made available, and in what amounts.

W. F.

Peking. Apr. 9.

Mr Townley

No 138

(ref. L.O. No 42 of Feb. 13)

recd 23 May. 1905 2 inclos.
by bag.

Indemnity: Northern Railways
claim.

to detailed statement.

Princ (China)

And consider in

print.

Feb

442 Ch. L.

	Inside Mail			Outside Mail			Total					
	£	s	d	£	s	d	£	s	d			
Jardine Matheson & Co As per Statement £ 19,517 16 5 and Tails 22,057.07 2 7 = £ 3,165 6 0	3117	2	5				3117	2	5			
Thames Iron Works Interest for 3 Years from 1 st July 1900 to 1 st July 1903 on their 4% } £ 3832.9 11 3 1/2% per Annum (for material landed at Tientsin)					402	8	0	402	8	0		
Saion Daniel & Co Interest on their 4%	175	19	10				175	19	10			
John Birch & Co Ltd Interest on their 4% for £2,500 for wharf dues for timber stored } in England for 3 Years from 1 st July 1900 to 1 st July 1903 3 1/2% per Annum }	262	10	0				262	10	0			
MacKenzie & Co For Boilers Tubes & Lundry materials etc 3/4 Cheng wu	756	13	4				756	13	4			
American Trading Co For 100 Glass Window glass	69	5	9				69	5	9			
Arnhold Karberg & Co For 1291 tons Japanese coal delivered at Tientsin				1067	4	7	1067	4	7			
Ching Shan Bank For 3 Years Interest @ 4% per Annum from 1 st July 1900 to 1 st July 1903 } on Tails 15,040.00 = Tails 3164.40 2 7 1/2					452	0	0	452	0	0		
The Lung Sheng Bank For 3 Years Interest @ 4% per Annum from 1 st July 1900 to 1 st July 1903 } on \$8,000.00 = Interest \$ 1120.00 @ 5 1/2% £1					148	0	0	148	0	0		
Freight & Store & Wharf Charge on Cargo stored in Shanghai & brought to Tientsin } in December 1902 by S.S. Ningpo & Changshou }					4350	14	5	4350	14	5		
Summary of Claims of Engineering departments as per Mr Kinder's Statement	209161	0	0	104010	0	0	313171	0	0			
Amount advanced by British Government, for replacement & renewal caused by Boxer damage	90,000	0	0				90,000	0	0			
Losses at Head Office, Tientsin	6714	0	0				6714	0	0			
Miscellaneous Claims as per Statement	618	11	6	470	0	0	1088	11	6			
Compensation to Staff, retained by B.R.A.	3173	0	0	789	0	0	3962	0	0			
Staff remaining in the East, but not employed by B.R.A.	511	0	0	178	0	0	689	0	0			
Staff which left the East, service not being required	2719	16	0	808	6	6	3528	22	6			
Wages & Compensation to coolies accompanying Siam Expedition \$4000.00	402	0	0				402	0	0			
Contingent Fund	7500	0	0				7500	0	0			
British & Chinese Corporation claim for losses at Kienpiao				8980	1	5	8980	1	5			
Estimated Cost of recovery of Railway lands							4000	0	0			
	7500	0	0	319680	18	10	121655	14	11	452886	13	9

Wm Marshall.
Secretary, Imperial Railway of North China.

Tientsin 17th December 1902.

M. T. Liang Esq.

Director Imperial Chinese Railways.

Present.

Dear Sir,

Railway Indemnity Claims.

Referring to our communication of 1st Ultimo and documents attached we now beg to hand you herewith an amended Statement- of Telegraphic Expenditure which please substitute for the one rendered.

Our claims are now summarised as follows:-

Amounts due for Material lost through the Boxer troubles in 1900 together with interest thereon as per separate statement

Tael. 20344.80

Expenditure on telegrams as per Statements attached

£ 1,981.16.5 and

1712.90

British & Chinese Corporation's Claim for losses at Nanpiao less amount allowed by H.B.M. Claims

Commissioner £ 8980. 1.5

£ 10931.17/0 and

Tael. 22057.70

We are, Dear Sir,

Yours faithfully,

(Signed)

JARDINE, MATHESON & CO.

7-1961 23

Summary of Amounts due by the Imperial Chinese Railways to the undersigned on account of Material imported under their Indents and lost in consequence of the Boxer troubles of 1900- also of Interest as per Statements attached.

A/c	Indent	No.	Tls:
"		32	4457.31
"		38	694.17
"		1605 (part)	1612.27
"		1605/1606	5174.39
"		1610	338.58
"		1676	
		£ 619.14.9 at $2\frac{1}{4}\frac{7}{16}$	5230.31
"	Various		
	Delivered to B.R.A.		2637.77
Total Tls: Tals			20344.80

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

W. S. Matheson

JARDINE, MATHESON & CO.

JARDINE, MATHESON & CO.

5 cases Split & Cotter pins	£ 47. 7. -
53 cases Iron Rivets	332. 5. -
3 cases Brass Sheets	
3 " Copper "	
2 " " Tubes	
1 " " & Brass Tubes	379.12. 1
Freight & B/Lading	£ 36. - 4
Insurance on £ 770 at 12/6 & Stamp	" 4.18. 3
Bill Stamp	" 7. -
	43. 5. 7
	£ 702. 9. 6
at Ex: $\frac{2}{4} \frac{3}{5}$	Tls: 5941.71
plus interest from 10.1.00 to 31-12.02 =	
995 days at 7 o/o per annum	" 1133.61
	Tls: 7075.52

Loss amounts received from the B.R. Administration
for portions of above Material taken over by them
plus interest calculated thereon as per memo: at foot } 2618.21
Balance due by the Imperial Chinese Railways Tals 4457.31

--- Memo. ---

1901.			
October	Amount received from B.R.A.	Tls. 1339.24	
	add- Interest from 1.11.01		
1902	to 31.12.02 = 14 months & 7 o/o	109.37	Tls: 1448.61
April	Ditto - Ditto -	Tls. 1117.45	
	Add- Interest from 1.5.02		
	to 31.12.02 = 8 months at 7 o/o	52.15	1169.60
		Tals	2618.21

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Per

2 Cases Crucibles £ 32.15. 3
 2 " Saws & Cutters " 56.11. 0
 2 " Vices " 1.12. 6
 1 " Ratchet Braces " 64. 3. 5
 2 " Jacks " 53.16. 0
 1 " Letter Stamps " 1. 1. 0
 Bulls Eyes " 7. 8
 Freight & S/Lading £ 11.17. 3
 Insurance on £ 240 " 1.10. 6
 Bill Stamp " 2.0 " 13. 9. 9
 £ 222. 4. 1
 at Ex: $2\frac{1}{4} \frac{3}{8} =$ Tls: 1879.44
 Plus Interest from 19.7.00 to 31.12.02 = 395 days a 7 c/o " 322.42
 Tls: 2201.86
 Less Amounts received from the B.R.A. for portions of above
 taken over by them and Interest calculated thereon as per
 Memo: at foot } 1507.69
 Balance due by Imperial Chinese Railways Tls: 694.17
 -- Memo.--
 Arch Amount received from B.R.A. Tls: 1407.20
 Add- Interest from 1.4.02 to 31.12.02
 = 9 months at 7 c/o 73.88 Tls: 1481.08
 Just Ditto Ditto Tls: 26.-
 Add Interest for 4 months " .61 " 26.61
 Tals 1507.69
 E. & O. E.
 Tientsin 1st November 1902.
 JARDINE, MATHESON & CO.
puta Surin

2 Cases Crucibles £ 32.15. 3
 2 " Saws & Cutters " 56.11. 0
 2 " Vices " 1.12. 6
 1 " Ratchet Braces " 64. 3. 5
 2 " Jacks " 53.16. 0
 1 " Letter Stamps " 1. 1. 0
 Bulls Eyes " 7. 8
 Freight & S/Lading £ 11.17. 3
 Insurance on £ 240 " 1.10. 6
 Bill Stamp " 2.0 " 13. 9. 9
 £ 222. 4. 1
 at Ex: $2\frac{1}{4} \frac{3}{8} =$ Tls: 1879.44
 Plus Interest from 19.7.00 to 31.12.02 = 395 days a 7 c/o " 322.42
 Tls: 2201.86
 Less Amounts received from the B.R.A. for portions of above
 taken over by them and Interest calculated thereon as per
 Memo: at foot } 1507.69
 Balance due by Imperial Chinese Railways Tls: 694.17
 -- Memo.--
 Arch Amount received from B.R.A. Tls: 1407.20
 Add- Interest from 1.4.02 to 31.12.02
 = 9 months at 7 c/o 73.88 Tls: 1481.08
 Just Ditto Ditto Tls: 26.-
 Add Interest for 4 months " .61 " 26.61
 Tals 1507.69
 E. & O. E.
 Tientsin 1st November 1902.
 JARDINE, MATHESON & CO.
puta Surin

6	Kegs Rivets		£ 22. 2. 6
120	Iron Tubes		" 97. 4. 3
1	Cask Bends &c		" 150. 0. 0
1	-- Flanges &c		" 1.12. 6
137	Steel Tank Plates		" 195.12. 6
	B/Lading & Freight	£ 54.6.0	
	Insurance on £ 465 &	" 21.9.5	
	Bill Stamp	" 4.4.6	
			£ 67. 9.11
			£ 425. 1. 5

at Ex: 2/4 = Tals 3595.21
 plus Interest from 9.4.00 to 31.12.02 - 996 days a 7 o/o " 659.33
 Tls. 4254.54

Less Amounts received from the British Railway Adminis-
 tration for portions of above Material taken over by them
 and Interest calculated as per memo: at foot " 2642.27
 Balance due by the I.C.Railways Tals 1612.27

--- Memo.---

1901
 October Amount received from the B.R.A. Tls: 425.92
 & Interest for 16 months from 1.9.01 to 31.12.02 " 39.75
 Tls: 465.67
 July Ditto Ditto Tls: 1980.25
 & Interest for 17 months
 from 1.8.01 to 31.12.02 " 196.37 " 2176.60
 Tals 2642.27

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Handwritten signature

18 Cases Bolt Ends		£ 96.10. 0
16 " Weighing Machines		" 97. 6. 3
97 Bars Angle Steel		" 27.19. 1
50 Cases Galva Iron		" 401. 5. 2
40 Kegs Brown Paint Oak		
48 Drums drying Oil thinnings		
6 -- Turpentine		105.-. 8
1 Case Chalk		2. 3. 0
1 -- Pump Leathers		
1 -- Padlocks & Snaps		11. 1.10
1 -- Sledge Hammers		37.17. 2
13 Bundles Miners Drill Steel		
Freight & B/Lading	£ 102. 4.10	
Insurances &c	" 6.12. 9	108.17. 7
		£ 886. 0. 9
Ex: $2\frac{1}{4} \frac{3}{8} =$		Tls: 7464.24
Plus Interest from 10.4.00 to 31.12.02=995 at 7 o/o		" 1450.06
		Tls: 8924.30
Less amounts received from R.R.A. for portions of above		
Material taken over by them and interest calculated		
Hereon as per memo: at foot		" 3749.91
Balance due by the Imperial Chinese Railways		Tls: 5174.39

301

Memo:

301	Amount received from R.R.A.	Tls: 356.58	
	Interest for 13 months from		
302	1/12/01 to 31/12/02 at 7 o/o	" 27.05	Tls: 363.63
	Ditto Ditto	Tls: 3216.19	
	& Interest for 8 months from		
	1.5.02 to 31.12.02 at 7 o/o	" 150.09	" 3366.26
			Tals 3749.91

S. & O. S.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Per

S. O. S.

12 - 1961 - 23

I.C.R. Indent No.1610 London Invoice No.89 due 10th April 1900.

2 Cases Drawing Material		£ 60. 5. 1
Freight & B/Lading	£ 1.13.11	
Insurance on £ 70 & Stamp	9	
Bill Stamp	1	£ 2. 3. 11
		£ 62. 9. 00

at Ex: $2\frac{1}{4} \frac{5}{8}$ =

Tals 528.21

Plus Interest from 10.4.00 to 31.12.02

= 995 days at 7 o/o

" 100.79

Tals 689.00

Less amount received from B.R.A. as per memoist foot

" 290.42

Balance due by the I.C.Railways

Tls: 338.58

--- Memo. ---

1902

April Amount received for part of above from

the B.R.A.

Tls: 277.47

Add- Interest from 1.5.02

to 31.12.02 = 8 Months at 7 o/o

" 12.95

Tls: 290.42

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

John S. Jardine

13-1961

Copy

Imperial Railways of North China.

to the undersigned

Dr. _____

For amount due on account of 60 lbs Rails delivered ex "Forestdale" and
"Macedonia" at Sin-Ho 20/6/01 under Indent No.1576 & Contract of 21/9/99.vi

4701 Rails = Tons	1252. 1. 3.17	at £ 7.16. 1½	= £ 9772.17. 3
Less Sales	3600	--	696. 8. 2. 9
	<u>2101</u>	Tons	<u>555.13. 1. 9</u>
			× £ 4337. 1.10

Plus interest at 7 o/o per annum as follows:-

on £ 2195. 4. 4 from 20/5/01 to 20/9/01 = 3 months £ 38.8. 4

3240.11. 1	"	--	20/2/02	8	--	151.4. 6
------------	---	----	---------	---	----	----------

4337. 1.10	"	--	20/11/02	17	--	430.1.11	× 619.14. 9
------------	---	----	----------	----	----	----------	-------------

£ 9772.17. 3						<u>£ 4956.16. 7</u>
--------------	--	--	--	--	--	---------------------

at Ex: $2\frac{1}{4} \frac{7}{16}$ = Tals 41650.38

E. & O. R.

Witnessed 8th October 1902.

JARDINE, MATHESON & CO.

Per

Edw. Matheson
per *London*

Outstanding £ 619. 14. 9 =

Tals 5230 31

14-1961 Z 3

No. of Pkgs	Material Vessel	Am't of Invoice	Due date	When paid	Period	Interest
17	Rivets " Ulysses "	788.47	9/5/00	31/10/01	541 days	81.81
34	" " "	1307.88	10/4/00	31/10/01	570 "	142.97
1	Belt Ends	"				
1	Sledge	do. "	29/8/00	29/11/01	458 "	12.75
3	Hammers					
3	Zinc Slates Marburg	176.94	14/6/00	11/3/02	636 "	21.83
4	Vices Ulysses					
1	Steel Springs	652.63	29/8/00	11/3/02	560 "	70.09
24	Vices Machaon					
1	Ratchet Braces	977.81	19/7/00	11/3/02	601 "	112.70
2	Beltling	908.29	14/7/00	11/3/02	606 "	105.56
217	Shovels Picks etc Ulysses	3222.40	29/8/00	11/3/02	588 "	363.38
11	Weighing Machines Machaon	1065.30	19/7/00	11/3/02	601 "	122.79
1	Crucibles	--				
1	Saws & Cut	375.11	19/7/00	11/3/02	601 "	43.24
192	Sundry Ulysses					
	Material Phrrhus	3428.07	10/4/00	12/5/02	763 "	501.63
2	Drawing Material	269.39	10/4/00	12/5/02	763 "	39.42
25	Rivets Pyrrhus	1084.90	10/4/00	12/5/02	763 "	158.75
3	Domes Ulysses	1041.06	9/4/00	12/5/02	764 "	152.54
176	Bars Iron Stenter	622.60	23/6/00	23/6/02	730 "	87.16
2	Soap Pyrrhus	69.81	9/5/00	12/5/02	826 "	11.06
137	Tank Plate Ulysses	1908.17	9/4/00	12/5/02	856 "	313.25
1342	Bars Iron Pyrrhus	3124.83	9/5/00	12/5/02	826 "	495.
1	Letter Stamps Machaon	12.56	19/7/00	12/5/02	765 "	1.84

Tls: 2837.77

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

E. O. E.

15-1961 23

Period Interest	When Paid	Am't of Due date	Invoice	Material Vessel	Ref No.
31.81	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
14.27	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
12.73	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
21.83	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
70.03	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
112.70	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
108.26	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
263.28	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
122.73	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
43.24	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
201.68	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
30.43	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
128.73	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
122.24	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
27.16	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
11.06	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
213.23	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
462.	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01
1.84	31/10/01	31/10/01	31/10/01	31/10/01	31/10/01

Statement Int November 1902

Ref No 2

Thames Iron Works

To Interest for three years
from 1st July 1900 to 1st July 1903
on Rev account £3882.9.4
@ 3 1/2 per ann for material
lended at 4 1/2 per ann.

£402. 8. 0

(signed) Thames Iron Works

Fearon Daniel & Co.

To Interest on their account
from 5th April 1901, for
677 packages Steel bars,
angle, plate and rivets
Shed at this Co.

£175. 19. 10

Ref 104.

104

John Birch & Co. Ltd

To Interest on their account
for £2500, for wheels and
tires for leaders, stored
in England, for 3 years from
1st July 1900 to 1st July 1903
@ 3 1/2 per ann.

£ 262. 10. 0

Mackenzie & Co

For Boiler tubes, boiler covering
as Ch. Geo, on account
of Ind. A. 1599.
599 package.

£ 756 13 4

Ref 106 106 9
Tientsin 4th December 1899.
COPY.
The Imperial Chinese Railways of North China.
Dr to The American Trading Co.
100 cases 16 X 12 Window glass at Tls: 4-85-- Tls: 485-0
The American Trading Co.
(Signed)
Agent.
Feb 485.⁰⁰
769. 5. 9.
19th 1961 23

Ref 107.

copy

Arnhold Karberg Co.

Contract 15th May 1900.

1291 tons Japanese Coal
delivered along side Railway
Wharf Peking @ \$7.85
per ton

\$10.134 35

Interest 15th June to 25th

Sept. 1901, 102 days @ 6%

169 92

\$10.304 27

@ 7.5

Total

7470 60

Total = £ 10.67 47

Ref No 8

Chin Shan Bank

To 3 years Interest @ 7%
 per ann from 1st July
 1900 to 1st July 1903 =
 Total 15,070.-

Rs 3164 70
 @ 7% £ 452 0.0

Ref No 9.

To Fung Sheng Bank

To Interest @ 7% per ann
from 1st July 1900 to 1st
July 1905 on \$8,000 =

\$1480.00

@ 7/10 = £ 148. 0. 0

Copy

No 10

109A
Voucher No. 126

Tientsin 30th December 1902.

Imperial Railways of North China

Dt to Buchheister & Co. Ltd. Tientsin

For the following viz:

Charges on Railway Material shipped
from Shanghai to Tientsin per 3/3.
Kings & Chang Chow.Charges in Shanghai as per Statement No 1
do do do No 2£ 26391 06
9572 62

Charges in Tientsin:

Taku Bay & Lighter Co. Voucher No.

10 \$ 3050 35

do 11 322 90

Interest Dec 18th to 30th 13 days 9 60

Telegrams & Postages to Kings 75 00

do to Chang Chow 15 10

Consular & Captain of Kings

3/2 150 @ 101 1/2 147 72

Commission 1 1/2%

\$ 3657 75 £ 36026 56
54 87 540 39

\$ 3712 62 £ 36566 95

Rate of exchange 71.4 = \$1 @ 2 1/2%

\$ 294 1 6 £ 4056 12 11

Received payment

(Sgt) Buchheister & Co. Ltd.

31 Dec 1902

294 1 6

4056 12 11

4350 14 5

23 - 1961 23

Imperial Railway of North China
December 30th 1902

Imperial Railway of North China
To the Receiver of the Imperial Railway of North China

For the following viz:

Charges on Railway Material forwarded
for Ningpo to Hsinke Wharf
Storage Shanghai Hongkong Wharf Co.
as per Voucher No. 1.

1. Freight on account from 14th Nov. to 26th.
Nov. Approximate arrival of Steamer at Soke
\$10000-- at 74.1125.
Balances of freight to be settled on Completion
of discharge, Voucher No. 8.
Marine Insurance £30000--
Shanghai Telegrams \$52.48. @ 74.1125.
Bill Brokerage 1/8 %.

Received payment
(Sgd) Receiver of the Imperial Railway of North China

copy

December 30th 1902

Imperial Railway of North China
To the Receiver of the Imperial Railway of North China

For the following viz:

Charges on Railway Material forwarded
for Ningpo to Hsinke Wharf
Storage Shanghai Hongkong Wharf Co.
as per Voucher No. 1.

Wharfage and Night & Sunday time
Stevedores

Freight on account from 14th Nov. to 26th.
Nov. Approximate arrival of Steamer at Soke
\$10000-- at 74.1125.
Balances of freight to be settled on Completion
of discharge, Voucher No. 8.
Marine Insurance £30000--
Shanghai Telegrams \$52.48. @ 74.1125.
Bill Brokerage 1/8 %.

Received payment
(Sgd) Receiver of the Imperial Railway of North China

£ 9147 60
273 90
848 90
6419 40
626 00
633 90
260 79
7738 90
666 67
29 05
£ 26652 11
33 82
26685 43
26291 06

@ 1/8 % 116.

Continued on back of page 100

Continued on back of page 100

Continued on back of page 100

Imperial Chinese Railway

Summary of Claims for Indemnity
Engineering Department

Schedule A. Rolling Stock

do	B.	Tientsin to Peking
do	C.	Tientsin to Kungshing
do	D.	Outside Wall
do	E.	Telegraph
do	F.	Bridge Works

L	C	A
45,900	0	0
144,380	0	0
7,480	0	0
61,333	0	0
11,150	0	0
42,928	0	0
\$ 313,171	0	0

(signed) C. W. M. de
Feb 9th 1903

- Note. This claim does not include
- (1) Compensation to Staff
 - (2) Destruction of Road Office, Tientsin
 - (3) Cost of storage & re-shipping material from Engineer's Office
 - (4) Interest payable on debt.

Indemnity claim for losses to cars
Locomotive and Shops.

Valuation of Rolling Stock on
June 1900, see attached report
16 Mud cars, I. C. R. type
200 German do " "

£345,000	0	0
2,040	0	0
8,000	0	0

Total	355,040	0	0
-------	---------	---	---

Re-valuation of same in October
1902, see attached report.
59 Mud cars, I. C. R. type
92 German do " "

374,300	0	0
2,360	0	0
2,680	0	0

Total	380,340	0	0
-------	---------	---	---

Increased value £33,800.

Cost of repairing & rebuilding
500 cars used by Russians
@ £30. each.

£15,000 0 0

do rebuilding 8 locomotives
destroyed by Boche P.R. line.
Repairs and rebuilding 26
locomotives partly destroyed
by Russians O.W.

9,600 0 0

Loss of materials for Car & Loco
building at Torgau (see
attached Schedule X) (page 10)

10,400 0 0

44,700 0 0

£19,700 0 0

Claim amount as above £19,700 0 0
Less: Increase in value of
stock due to building operations
during B.R.A.

33,800 0 0

Balance due £ 45,900 0 0

Indemnity claim for losses & damage to
Tientsin Peking Railway

80 Mils of 2nd track									
Rails 12,000 lms @ £7	£	84,000	0	0					
Sleepers.		30,000	0	0					
Ballast		12,000	0	0					
Plate laying		3,000	0	0					
Port & crossing		1,000	0	0					
Track									
Cost of renewals & repairs of									
girders, both tracks									
Floors of same. do do									
Destruction of Flagstaff Shops.									
Stores lost therein do									
No. 1 do do									
Water crane, pipe etc									
3 Iron Sodavans & trucks									
Losses of siding & crossing									
Locomotive store									
Extra length of shed (Lugrie)									
Repairs & renewals of stations									
L. C. P. Station & yard.									
Merchant Hoys therein									
Main line signals									
Electric tramway plant.									
Main road & steam Roller.									
	£	130,000	0	0					
		2,700	0	0					
		1,400	0	0					
		1,200	0	0					
		4,500	0	0					
		4,500	0	0					
		350	0	0					
		3,000	0	0					
		1,000	0	0					
		200	0	0					
		250	0	0					
		2,000	0	0					
		7,000	0	0					
		4,000	0	0					
		2,000	0	0					
		20,000	0	0					
		2,000	0	0					
	£	186,100	0	0					

Value of works carried out by B. R. A.
for Capital etc etc.

Chin New Extension
(Value of rail included)
12 Miles of Rail left in stock

£ 32,000 0 0

9,720 0 0

£ 41,720 0 0

Less: value of work done by
B. R. A. on Extension.

£ 186,100 0 0

41,720 0 0

£ 144,380 0 0

11/5

Indemnity claim for damage done inside
Wall between Tiki and Kuyeh.

Mr. Tucker Report and
estimate \$193,658.00
Extra new iron for
roofs 3,812.00

Mr. Marshall's statement
as to timber at Kishi
in 1900.
1900 value \$181,200 -
1902 " 169,000 -
Loss 12,200 - 12,200.00
\$ 209,770.00

Sale of 75 lb rail to Kishi
Railway \$120,000.00

Balance due \$89,770.00

⑨ #12 = £1. - £7,480.-

Indemnity claim for losses incurred Outside Wall.

Damage as per Mr. Cox's Report and Estimate	\$76,530.00				
Letting for Blake Crusher	2,500.00				
Rebuilding of defective piers of bridge	3,500.00				
Letting of Turntable V.K.	1,000.00				
	\$78,530.00			\$78,530.00	
Estimated value of work done by Russians for Capital reconstruction of Railway.				155,194.00	
Balance due				\$66,336.00	
Q 110 - L	\$66,333.00				

Indemnity claim for injury done
to Telegraph Dept.

Injury done Inside Wall
See Mr. Hwang's report. \$ 50,415.00

do do Outside Wall 38,905.00

Total. Each. 89,320.00

@ The - £ 1,1150.00

11/8

Schedule F.

118

Indemnity claim for losses incurred
at Shalika River Bridge Works

Loss of material, tool
and damage to plant
as per W. Howard's Report
and Shrike

\$42,928 0.00

11/9.

Approximate Value of Stock

Schedule X.

Tongue Workshops & Loco Sheds

12th Nov 25th year of the year
(25th Jan 1900).

Materials approximately received
from 25/1/00 to 7/4/00, to meet
the requirement for the erection of
New Rolling Stock J. W.
do do O. W.
Against Running store made to

Issue. February, March, April and
May 1900, £65,000 per month

Less. Value of Stock found on the
1st October 1900 \$1,131,371.00 @ 70%

Total

977,044.00

67,126.00

283,605.00

58,651.00

1,386,426.00

260,000.00

1,126,426.00

791,960.00

Total 334,566.00

@ 750 £44,702 0.0

See Schedule A. Sheet 2.

Advanced to British
Railway Administration
by British Government for
replacement and renewal
caused by Boxer damage

£90,000 0-0

Ref No 13

120A

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account.

Losses at Head Office, Tientsin.

Light Rails and Cars.	Taels 14,000.
Cost of replacing Title Deeds	<u>20,000.</u> \$4,000.
Furniture, Safes etc.	10,000.
Stationery. etc.	5,000.
Dollars and fractional coins.	<u>13,000.</u>
	<u>Taels 47,000.</u>

@ 70. Ex. =

2,857. 26. 74. 0. 0

36. 1961. 2

- 495³ @ 49 10.0

37	1961	Σ 3
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Ref 10 15
Schedule C.

Compensation to Staff which was retained in the east ready
for immediate reemployment under B. R. A. Service.

W.

Names.	Particulars.	Amount	Total
Barber. J.	5 months half pay from Aug. to Dec. at \$11.00. per month = \$55.00	55.00	
	House allowance 3 months at \$150	1350.00	1900.00
Cox. A. G.	Was paid full salary. House allowance 2 months at \$150	1350.00	
	Less \$350. drawn at Yinkow	-350.00	1000.00
Cheyne. J. J.	5 months half pay at \$10.00 = \$50.00 House allowance 3 months at \$150	50.00 1350.00	1950.00
Engstran. H.	3 months at half pay at \$13.00 per month = \$39.00 3 months House allowance at \$150	39.00 1350.00	1740.00
Polcy. J. E.	Received full pay 3 months House allowance at \$150	1350.00	1350.00
Harris. F. A.	5 months half pay at \$17.10. per month = \$85.50 3 months House allowance at \$150	85.50 1350.00	2235.00
Carried Forward		7	19065.00

Compensation to Staff which was retained in the east ready for immediate reemployment under R. A. Service.

Names.	Particulars.	Amount	Total.
Howard. W. G.	5 months pay at \$22.16.4 =	1108.82	1108.82
Jamieson. F. A.	5 half months pay	1350.00	1350.00
Kitching. F.	2 months at half pay	775.00	775.00
Kinder. C. W.	House allowance 9 months at \$150.	1350.00	1350.00
Leitch. W. O.	5 months half pay	1350.00	1350.00
Martin. J. C.	5 months half pay	1350.00	1350.00
Marshall. H. J. W.	5 months half pay	1350.00	1350.00
Moffat. J.	House allowance 9 months	1350.00	1350.00
Moore. J.	Do. do	1350.00	1350.00
No	5 months half pay	1350.00	1350.00

Claim for Compensation of Railway Staff.

Names.	Particulars.	Amount	Total.
Howard. W. G.	5 months pay at \$22.16.4 =	1108.82	1108.82
Jamieson. F. A.	5 half months pay	1350.00	1350.00
Kitching. F.	2 months at half pay	775.00	775.00
Kinder. C. W.	House allowance 9 months at \$150.	1350.00	1350.00
Leitch. W. O.	5 months half pay	1350.00	1350.00
Martin. J. C.	5 months half pay	1350.00	1350.00
Marshall. H. J. W.	5 months half pay	1350.00	1350.00
Moffat. J.	House allowance 9 months	1350.00	1350.00
Moore. J.	Do. do	1350.00	1350.00
No	5 months half pay	1350.00	1350.00

121

121

Schedule A. (Continued)

Office for Compensation of Railway Staff

Names.	Particulars.	Amount	Total
MacLellan, J.	3 months House allowance at \$150. per month went on leave home	450.00	450.00
Newmarch, I. J.	5 months half pay at \$225. - " \$125. - "	1250.00	2600.00
Rathmell, W.	House allowance 9 months at \$150. 5 months at half pay at \$250. 0. \$27. 10. 0.	1350.00	1625.00
Reckerby, J.	On full pay drawn from military		1350.00
Ricketts, D. P.	Paid by Military		
Regby, E. H.	House allowance 2 months at \$150. Received full pay	300.00	300.00
Roberts, H.	House allowance 9 months at \$150. Deduct \$350. drawn at Ying Kou	1350.00	1000.00
Sheriff, A.	3 months full pay at \$120. 2 do half pay at \$60	360.00	1200.00
Tuckey, T. W. T.	House allowance 9 months at \$90. Received full pay	810.00	
Wheeler, A.	No House allowance Fully paid		
	5 months half pay at \$27. 10. 0. \$47. 10. 0.	475.00	1325.00
	House allowance 9 months at \$150. pay to Oct. 26. by Military House allowance 5 months at \$150.	1350.00	1325.00

15/2

15/3

124

A. 2.

Names.	Particulars.	Amount	Total
MacLellan, J.	3 months House allowance at \$150. per month went on leave home	450.00	450.00
Newmarch, I. J.	5 months half pay at \$225. - " \$125. - "	1250.00	2600.00
Rathmell, W.	House allowance 9 months at \$150. 5 months at half pay at \$250. 0. \$27. 10. 0.	1350.00	1625.00
Reckerby, J.	On full pay drawn from military		1350.00
Ricketts, D. P.	Paid by Military		
Regby, E. H.	House allowance 2 months at \$150. Received full pay	300.00	300.00
Roberts, H.	House allowance 9 months at \$150. Deduct \$350. drawn at Ying Kou	1350.00	1000.00
Sheriff, A.	3 months full pay at \$120. 2 do half pay at \$60	360.00	1200.00
Tuckey, T. W. T.	House allowance 9 months at \$90. Received full pay	810.00	
Wheeler, A.	No House allowance Fully paid		
	5 months half pay at \$27. 10. 0. \$47. 10. 0.	475.00	1325.00
	House allowance 9 months at \$150. pay to Oct. 26. by Military House allowance 5 months at \$150.	1350.00	1325.00

[illegible]

Messrs. Irwin.		Amount	Total
Half pay June July 1900 to 20th Feb. '01. to date of D.R.A.			
Landing over to I.C.R. 7 months			
At 22s. per month 214s. 00		1488.75	1488.75
at 21s. 10d. per month 207s. 10d.		207.10	
6 months Notice allowance at 21s.		252.00	39.62 1/2
		Total	4155.60
Messrs. W.C.L.			
2 months 15 days of notice			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
Inside Ball 2170. 68	= £	317s. 0.0	
Outside do 7890. 33	= £	789s. 0.0	
Messrs. J.			
Employed by D.R.A.			
6 months half pay due in lieu of notice			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
Less 20s. drawn at Vintages		-20.00	187.10
Messrs. D.R.			
3 months half pay in lieu of notice			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
Messrs. D.R.			
2 months at half pay			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
3 months full-pay at 21s. 10d. per month 207s. 10d.		207.10	207.10
Messrs. Otto.			
3 months full-pay at 21s. 10d. per month 207s. 10d.		207.10	207.10
Messrs. D.R.			
3 months half pay			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
Messrs. R.D.			
3 months half pay			
at 21s. 10d. per month 207s. 10d.		207.10	207.10
		Total	4833.75
Inside Ball 1704. 12	= £	24s. 9.0	
Outside do 1779. 16	= £	78s. 0.0	
1683. 28	= £	689s. 0.0	

18

Handwritten over Social C.R. 7 months
 20th Feb. '41. In date of B.R.A.
 Half pay June July 1941 1000 10

U.S. DEPARTMENT OF AGRICULTURE

18307

0.0. 41.6	10	81.00	10	10.00	10
0.0. 42.5	10	82.00	10	10.00	10
0.0. 43.4	10	83.00	10	10.00	10

Ref. No. 16

Schedule C.

Staff remaining in East but not

employed by I.C.Ry or B.R.A.

Sr.	Names	Particulars.	Amount	Total
	Brown. H.H.	Five months in lieu of notice at \$12.15.4= 232.15.8	625.00	625.00
	Clarks. C.	6 months House allowance at \$150	900.00	1525.00
	Dunn. E.C.A.	Five months in lieu of notice at \$16.15.4= 233.6.0	833.33	633.33
	Hicks. J.	Was not asked to remain.		750.00
	Dixon. A.	Required by reemployed		
	Gibson R.G.	Reemployed by B.R.A.		
	Dunn. E.C.A.	5 months half pay due in lieu of notice at \$16.15.4= 233.6.0	833.33	
		Less \$350. drawn at Yingkou	-350.00	483.33
	Griffeth. D.P.	5 months half pay in lieu of notice at \$28.16.8= 164.3.4	141.62	141.62
	Knowles. G.S.	2 months at half pay at \$12.15.8= 235 -"	-250.00	
		3 months full pay at \$25 \$75-"	-750.00	1000.00
	Krier. Otto.	3 months full pay at \$200.	-600.00	600.00
	Norregard. B.W.	5 months half pay at \$18.15.8= 203.15.0	-937.50	937.50
	Palmer. R.J.	5 months half pay at \$25.5.0. \$46.5.0.	-462.50	462.50
		5 months pay in lieu of notice	Total	6883.25
		Inside Ball \$704.12	511.00	
		Outside do 1779.16	178.00	
		\$6883.28	689.00	

employed by I.O.O.F. or H.B.A.
Staff remaining in Unit but not

[illegible]

204.15
 1551
 204.15

Staff which left the Eastnot required to remain.

Names.	Particulars	Amount	Total
Bone. T.	Five months full-pay in lieu of notice at \$18. per month \$90.00	900.00	900.00
Clarke. C.	5 months full-pay at \$19.00 = \$95.00	1350.00	1350.00
Clarke. J.	5 months full-pay at \$15.00 = \$75.00	750.00	750.00
Carrie. G. <i>Letter of 2/10/03</i>	5 months full-pay <i>6 months pay 7 months pay</i> at \$46.16.0 = \$230.8.0	2301.62	6500.00 2301.62
Dunn. E.C.A.	5 months full-pay in lieu of notice at \$19.00 = \$95.00 deduct \$350. drawn at Ying-kou	900.00 -350.00	550.00
Dorrell. H.	5 months House allowance at \$19.00 = \$95.00	900.00	900.00
Emerson. A.	5 months at half pay at \$11.10.0. per month = \$55.5.0. 5 months House allowance at \$150. Less \$350 was retained at Yingkou for 6 months	575.00 900.00 -350.00	1125.00
Early. W.H.	5 months half pay at \$12.10.0. \$62.10.0.	625.00	625.00
Franklin. H.	5 months full pay at \$18.10.0. \$90.10.0.	925.00	925.00
Gennill. J.	5 months pay in lieu of notice at \$56.6.8 = \$291.13.4. Less \$350. drawn at Yingkou	2916.62 -350.00	2566.62
Hurst. F.	Ask to return to China refuse to do so 8 months at half pay at \$14.11.8 = \$113.4. Carried forward	<i>paper blank</i> 4 621.18.4 1166.63	6319.16 1166.63
		43 1061 23	13140.87

Names.	Particulars	Amount	Total
	Brought forward		14140.82
Hancock. G.W.	5 months full pay in lieu of notice at £23.6.0. = £110.0.0.	1100.00	1100.00
Hoyle. A.B.	5 months at half pay at £18.15.0. = £93.75.0.	937.50	937.50
Jackson. J.W.	5 months full pay at £23.6.0. = £110.0.0.	1100.00	1100.00
	Less £350. drawn at Yinkou	350.00	1316.62
Mackenzie. A.C.	5 months at half pay at £16.13.4. = £80.6.8.	806.33	806.33
	Less £350. paid at Yinkou	350.00	456.33
Rohke. O.	5 months half pay at £2. = £45.	450.00	450.00
	Less £350. drawn at Yinkou.	350.00	100.00
Simmon. G.	5 months half pay at £12.10/- = £60.10/-	600.00	600.00
	Less £350. drawn at Yinkou	350.00	250.00
Statham. E.E.	Probably 2 months pay due 5 months pay £166.6.4 at £23.6.0. = £110.0.0.	1100.00	1100.00
Florman. W.	5 months full pay at £20. = £100.	1000.00	1000.00
Packham. R.	Was engaged but never served on Railway & during trouble was paid passage. Say 2 months pay as compensation at £12. = £24.	240.00	240.00
	half months House allowance	75.00	315.00
Stafford. W.R.	5 months full pay in lieu of notice at £18.10.0. = £90.5.0.	905.00	905.00
Strong. J.	Six months pay in lieu of notice decreased relative claim at £25.0.0. = £150.0.0.	1500.00	1500.00
	Carried forward		21888.84
			31249.89
			27

Names.	Particulars	Amount	Total
	Brought forward		21880.94
Symonds. T.	5 months full pay in lieu of notice at £26.0.0.=£130.0.0.	1300.00	1300.00
Terris. R.	5 months full pay in lieu of notice at £30.0.0.=£150.0.0.	1500.00	1500.00
Turner. R.S.	5 months half pay at £20.5.0=£231.2.6.	231.25	231.25
	Total		23400.19 35281.14
<p>Inside Wall $\frac{1}{2}$ 27197.91 = £ 2719.16.0</p> <p>Outside do 8083.22 = 808.6.6</p> <p>$\frac{1}{2}$ 35281.14 = £ 3528.2.6</p>			

Ref No. 18

1930	1930	1930	1930
1931	1931	1931	1931
1932	1932	1932	1932
1933	1933	1933	1933
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2003	2003	2003	2003
2004	2004	2004	2004
2005	2005	2005	2005
2006	2006	2006	2006
2007	2007	2007	2007
2008	2008	2008	2008
2009	2009	2009	2009
2010	2010	2010	2010
2011	2011	2011	2011
2012	2012	2012	2012
2013	2013	2013	2013
2014	2014	2014	2014
2015	2015	2015	2015
2016	2016	2016	2016

Wage Compensation

U -

For Wage Compensation

to coolie accompanying

Seymour Expedition

\$4019.98 @ 402.00

Ref No. 19

W- Contingent Fund.

For this amount set
aside for Contingent
purposes.

Tals	50,000 ⁰⁰	-	2,500	0.	0
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Ref No. 20

British Overseas
Corporation

W.

British Overseas Corporation

claim for losses at

Mar. 1940. See Ref No. 1

£8980 1. 5

7

IMPERIAL CHINESE RAILWAYS

133

Dr.

TO BRITISH & CHINESE CORPORATION LTD.

W-1
For amount of claim on account of losses in connection
with the Boring operations at Nanpiao as presented to
H.B.M. Claims Commissioner

£ 12012. 7. 4

Plus claimed for damages -

i.e interest on money, expenses of general &
head office, Administration charges
postponement of profit &c, say 15 c/o

" 1801.17. 1

£ 13814. 4. 5

Less amount of Indemnity allowed by Claims Commissioner
under the assumption that a portion of the Plant
remained uninjured.

" 4834. 3.

£ 8980. 1. 5

(This Claim is based upon information of the total destruction of every
thing by Native Soldiers or Brigands early in 1901 - see copies of corres-
pondence attached)

JARDINE, MATHESON & CO.

Per

9/10/14

9/10/138

1903

for Mr. L. J. Matheson

Dr.

TO BRITISH & CHINESE CORPORATION LTD.

For amount of claim on account of losses in connection with the Boxer operations at Hankow as presented to

3.12.1901

H.M. Claims Commissioner

has obtained for damages

1. Interest on money, expenses of General &

Head Office, Administration charges

1.12.1901

postponement of profit etc, say 10%

3.12.1901

has amount of indemnity allowed by Claims Commissioner

where the assumption that a portion of the plant

3.12.1901

remained undamaged

3.12.1901

This claim is based upon information of the total destruction of every

thing by Native Soldiers or Bandits early in 1901 - see copies of corres-

pondence enclosed

This item of £ 4000 represents, according to Mr Kinder's estimate, the cost of a resurvey of the Railways lands.

This resurvey is made necessary by the loss or destruction of all the Title Deeds which were kept on the safes at the Head Office in Tientsin. These Deeds were the Railway's sole evidence of title as there seems to have been no official registration of the lands from time to time acquired by the Railways.

The British & Chinese Corporation as Mortgagee is interested in the reestablishment of the title for the purposes of the security, and this can be accomplished only upon the basis of a resurvey.

It is submitted therefore that the expense in question is a direct consequence of the Boxer uprising and may properly be included in the Indemnity Claim.

135

4 April, 1903.

Sir,

Referring to the joint communication from Mr. Hewat and myself of 27 April last, I now beg to hand you herewith for the information of H.M. Government a copy of the Railway Claim as amended, amounting to £452,836. 13. 9.

The Administration holds of course the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by H.M. Government to examine and deal with same.

The Administration would however suggest the expediency of the examination being conducted in Tientsin where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that in consequence of the large expenditure already incurred in connection with the work of restoring the line to its ante-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of 26th. January, I venture to express the hope that H.M.Government will come to a decision as soon as possible when, and how, the final claim allowed will be made available, and in what amounts.

I understand that owing to questions at issue between the Powers and Chinese Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of Bonds representing the British

share

alter Townley Esq.

AC. AC. AC.

P R I N T I N G.

4-1961 23

share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted and the settlement of the Railway Claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration., not only with regard to the claims of firm and individuals for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that H.M.'s Government will see its way to arrange for the examination and settlement of the Claim as soon as possible.

I have etc.

(Sgd.) Ed. Cousins.

Representing British & Chinese Corpn. Ltd.

1961 23

2
in
h2 Tomley
h2 138
9 April
1903.

AFFAIRS OF CHINA.

[May 23.]

CONFIDENTIAL.

SECTION 3.

[No. 1.]

Mr. Townley to the Marquess of Lansdowne.—(Received May 23.)

(No. 138.)

My Lord,

Peking, April 9, 1903.

I HAVE the honour to transmit herewith to your Lordship, in original, and with reference to your despatch No. 42 of the 13th February last, a detailed Statement of the Northern Railways claim as furnished to me by Mr. Cousins.

Your Lordship will perceive that the total claim for both intra- and extra-mural lines amounts to 448,836 13s. 9d., to which will have eventually to be added a sum of 4,000L., the estimated cost of the resurvey of the line which will have to be made in order to establish the titles to lands of which the deeds were lost in the destruction of the head office at Tien-tsin. The above-mentioned sum, divided, I am told, as well as may be between the two lines, is made up of:—

						£	s.	d.
Intra-mural line	..	..	..	..	..	319,680	18	10
Extra-mural line	..	..	..	..	..	121,655	14	11
Contingent fund	..	..	..	..	..	7,500	0	0
Total	..	..	..	..	..	448,836	13	9

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his despatch No. 320 of the 29th October last, has caused him to place the Engineering Department's claim for damage to that line at 104,010L. Only a small portion of this last figure covers damage to rolling-stock, and that chiefly applicable to engines, since it was not possible to make a full separate allocation of rolling-stock to the one line or the other, and where separation has not been made the rolling-stock and supplies have been attributed to the intra-mural line.

Mr. Cousins tells me that it is possible that the sum of 300,000L. claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which, he is given to understand, amounts to about 150,000L., although, as far as he can ascertain, no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to your Lordship at a later date.

From the appended Statement, it would, however, appear that the sum of 500,000L. claimed on account of the intra-mural line would prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr. Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its rolling-stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which, however, in the shape of bonds, is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to your Lordship upon its receipt.

Mr. Cousins points out that the sum of 30,000L. referred to in your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which your Lordship will see that the sum of 90,000L. advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr. Cousins and Mr. Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of 7,500L. seems to be a necessary precautionary measure to take, and if the money is not required it will not be used.

[1961 z—3]

B

References 15, 16, and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although, in most cases, the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon 9,000*l.* at which the amount of possible compensation has been fixed may be somewhat reduced. A Notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the Notice. It is clear that, without such public announcement, the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement of claims if the document is printed, as press of work has prevented a copy being made here.

I have, &c.

(Signed) WALTER TOWNLEY.

P.S.—Since writing the above, I have received an amended statement from Mr. Cousins, which includes the 4,000*l.* on account of resurvey, together with a Memorandum explaining the necessity for the same. In transmitting these documents to your Lordship, I have also the honour to inclose copy of a letter just received from Mr. Cousins, covering the railway claim, pointing out that the Railway Administration is seriously hampered by want of funds, and expressing a hope that His Majesty's Government will come to an early decision as to when, and how, the final claim allowed will be made available, and in what amounts.

W. T.

Inclosure 1 in No. 1.

Mr. R. Cousins to Mr. Townley.

Sir,

Tien-tsin, April 4, 1903.

REFERRING to the joint communication from Mr. Hewat and myself of the 27th April last, I now beg to hand you herewith, for the information of His Majesty's Government, a copy of the railway claim, as amended, amounting to 452,836*l.* 13*s.* 9*d.*

The Administration holds, of course, the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by His Majesty's Government to examine and deal with same.

The Administration would, however, suggest the expediency of the examination being conducted in Tien-tsin, where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that, in consequence of the large expenditure already incurred in connection with the work of restoring the line to its anti-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of the 26th January, I venture to express the hope that His Majesty's Government will come to a decision as soon as possible when, and how, the final claim allowed will be made available, and in what amounts.

I understand that, owing to questions at issue between the Powers and Chinese Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of bonds representing the British share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted, and the settlement of the railway claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration, not only with regard to the claims of firms and individuals, for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that His Majesty's Government will see its way to arrange for the examination and settlement of the claim as soon as possible.

I have, &c.

(Signed) ED. COUSINS,

Representing British and Chinese Corporation (Limited).

Inclosure 2 in No. 1.

DETAILED Statement of the Northern Railway Claim.

SUMMARY of Claim.

No.	Inside Wall	Outside Wall	Total.
	£ s. d.	£ s. d.	£ s. d.
1	5,117 2 5	402 8 0	5,519 10 5
2	175 19 10	262 10 0	437 29 10
3	262 10 0	756 13 4	1,018 23 4
4	69 5 9	1,067 4 7	1,136 10 6
5	..	452 0 0	452 0 0
6	..	148 0 0	148 0 0
7	..	4,350 14 5	4,350 14 5
8	..	10,410 0 0	10,410 0 0
9	..	90,000 0 0	90,000 0 0
10	..	6,714 0 0	6,714 0 0
11	..	470 0 0	470 0 0
12	..	789 0 0	789 0 0
13	..	3,902 0 0	3,902 0 0
14	..	689 0 0	689 0 0
15	..	3,428 2 6	3,428 2 6
16	..	402 0 0	402 0 0
17	..	..	..
18	..	..	..
19	..	..	..
20	..	..	..
21	..	..	..
Total	319,680 18 10	121,635 14 11	441,315 13 9

(Signed) H. J. W. MARSHALL, Secretary, Imperial Railways of North China.

Tien-tsin, December 17, 1902.

Dear Sir,

Referring to our communication of the 1st ultimo, and documents attached, we now beg to hand you herewith an amended Statement of telegraphic expenditure, which please substitute for the one rendered.

Our claims are now summarized as follows:—

Amounts due for material lost through the Boxer troubles in 1900, together with interest thereon, as per separate Statement	Taels. 20,344·80 1,712·90
Expenditure on telegrams, as per Statements attached	£ 1,951 16 5 and
British and Chinese Corporation's claim for losses at Nampiao, less amount allowed by His Britannic Majesty's Claims Commissioner	8,980 1 5
	10,931 17 10 and
	22,057·70

We are, &c.
(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

M. T. Liang, Esq., Director,
Imperial Chinese Railways.

SUMMARY of Amounts due by the Imperial Chinese Railways to the Undersigned, on account of Material imported under their Indents and lost in consequence of the Boxer troubles of 1900, also of Interest as per Statements attached.

A/c Indent	No.	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration	Total	Taels.
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		4,457·31
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		694·17
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		1,612·27
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		5,174·39
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		338·58
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		5,250·31
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		2,837·77
"	"	32	38	1605 (part)	1605/1608	1610	1616	619/ 14s. 9d., at 2s. 4½d.	Various, delivered to British Railway Administration		20,344·80

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 32 (part), London Invoice No. 68, due April 10, 1900.

	£	s.	d.	£	s.	d.
5 cases split and cotter pins
53 " iron rivets
3 " brass sheets
3 " copper sheets
2 " copper tubes
1 case copper and brass tubes
Freight and bill of lading
Insurance on 770L, at 12s. 6d., and stamp
Bill stamp
				38	0	4
				4	18	5
				0	7	0
				43	5	7
				702	9	6
At ex. 2s. 4½d.
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. per annum
			
Less amounts received from the British Railway Administration for portions of above material taken over by them, plus interest calculated thereon as per Memo. at foot
			
Balance due by the Imperial Chinese Railways
			
				4,457	31	

	MEMO.	Taels.	Taels.
1901. October. Amount received from British Railway Administration	1,339·24	..
Add—Interest from 1st November, 1901, to 31st December, 1902, = 14 months, and 7 per cent.	109·37	..
1902. April. Amount received from British Railway Administration	1,117·45	1,448·61
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent.	52·15	..
	1,169·60
Total	2,618·21

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin November 1, 1902.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indents Nos. 1605 and 1608 (part), London Invoice No. 87, due April 10, 1900.

	£ s. d.	£ s. d.
13 cases bolt ends .. .. .	.. .. .	96 10 0
16 .. weighing machines .. .. .	.. .. .	97 6 3
57 bars angle steel .. .. .	.. .. .	27 19 1
50 cases galvanized iron .. .. .	.. .. .	401 5 2
40 kegs brown paint oak .. .. .	.. .. .	.. .. .
48 drums drying oil thinnings .. .. .	.. .. .	.. .. .
6 .. turpentine .. .. .	.. .. .	.. .. .
1 case chalk .. .. .	.. .. .	105 0 8
1 .. pump leathers .. .. .	.. .. .	2 3 0
1 .. padlocks and snaps .. .. .	.. .. .	.. .. .
1 .. sledge hammers .. .. .	.. .. .	11 1 10
13 bundles miners drill steel .. .. .	.. .. .	37 17 2
Freight and bill of lading	102 4 10	
Insurances, &c.	6 12 9	
		108 17 7
		886 0 9
		Tsels.
At ex. 2s. 4½d. = .. .. .	.. .. .	7,494.24
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. .. .. .	.. .. .	1,450.06
		8,924.30
Less amounts received from British Railway Administration for portions of above material taken over by them, and interest calculated thereon as per Memo. at foot .. .. .	.. .. .	3,749.91
Balance due by the Imperial Chinese Railways .. .. .	.. .. .	5,174.39
		Tsels.
1901. Amount received from British Railway Administration .. .. .	.. .. .	356.58
Interest for 13 months from 1st December, 1901, to 31st December, 1902, at 7 per cent. .. .. .	.. .. .	27.05
		383.63
1902. Amount received from British Railway Administration .. .. .	.. .. .	3,216.19
Interest for 8 months from 1st May, 1902, to 31st December, 1902, at 7 per cent. .. .. .	.. .. .	159.09
		3,366.28
Total .. .. .	.. .. .	3,749.91

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 1610, London Invoice No. 89, due April 10, 1900.

2 cases drawing material	£ s. d.	£ s. d.
Freight and bill of lading .. .. .	.. .. .	60 5 1
Insurance on 70L and stamp	1 13 11	
Bill stamp	0 9 0	
	0 1 0	2 3 11
		62 9 0
At ex. 2s. 4½d. = .. .. .	.. .. .	528.21
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. .. .. .	.. .. .	100.79
		689.00
Less amount received from British Railway Administration, as per Memo. at foot .. .. .	.. .. .	290.42
Balance due by the Imperial Chinese Railways .. .. .	.. .. .	338.58
		Tsels.
1902. Amount received for part of above from the British Railway Administration .. .. .	.. .. .	277.47
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent. .. .. .	.. .. .	12.95
		290.42

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL Railways of North China to the Undersigned debtor.

For amount due on account of 60-lb. rails delivered ex "Forestdale" and "Macedonia" at Sin-Ho 20th June, 1901, under Indent No. 1576, and contract of 21st September, 1899, viz.:		£ s. d.
Less sales	Tons, cwt. qrs. lbs.	
4,701 rails = 1,252 1 3 17	.. .. .	9,772 17 3
3,600	696 8 2 9	5,435 15 5
2,101	555 13 1 9	4,337 1 10*
Plus interest, at 7 per cent. per annum, as follows:—		
On 2,196L 4s. 4d. from 20th May, 1901, to 20th September, 1901 = 3 months .. .. .	.. .. .	38 8 4
" 3,240L 11s. 1d. from 20th May, 1901, to 20th February, 1902 = 8 months .. .. .	.. .. .	151 4 6
" 4,337L 1s. 10d. from 20th May, 1901, to 20th November, 1902 = 17 months .. .. .	.. .. .	430 1 11
9,772L 17s. 3d.		619 14 9†
		4,956 16 7
		Tsels.
At ex. 2s. 4d. 7½ = .. .. .	.. .. .	41,650.38

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, October 8, 1902.

* Paid in London.
[1961 s--3]

† Outstanding 619L 14s. 9d. = 5,230.31 tsels.

D

STATEMENT, showing Interest due to the Undersigned on Railway Material taken over by the British Railway Administration.

Indent No.	Number of Packages.	Material.	Vessel.	Amount of Invoice.	Due Date.	When Paid.	Period.	Interest.
32	17	Rivets ..	"Ulysses"	Tsels. 788-47	May 9, 1900	October 31, 1901	Days 341	Tsels. 341
1605	34	Bolt ends ..	"	1,307-88	April 10, "	" 31, "	570	142-97
1608	1	Sledge hammers ..	"	145-16	August 29, "	November 29, "	458	12-75
	3	Zinc shales ..	"Marburg"	178-94	June 14, "	March 11, 1902	636	21-83
	4	Vices ..	"Ulysses."	632-63	August 29, "	" 11, "	560	70-09
38	24	Wired springs ..	"Machon."	927-81	July 19, "	" 11, "	601	119-70
	1	Batchet braces ..	"	908-29	August 14, "	" 11, "	606	105-46
1618	2	Shovels, picks, &c. ..	"Machon"	3,222-40	August 29, "	" 11, "	588	363-38
1617	11	Weighing machines ..	"Ulysses"	1,065-50	July 19, "	" 11, "	601	122-79
38	1	Crucibles ..	"Machon"	375-11	" 19, "	" 11, "	601	43-24
	1	Saws and cut ..	"Ulysses"	3,338-07	April 10, "	May 12, "	763	501-63
1605	192	Sundry ..	"Pyrrhus"	2,622-39	" 10, "	" 12, "	763	134-22
1608	2	Drawing material ..	"Pyrrhus"	1,084-90	" 10, "	" 12, "	763	138-74
1610	32	Rivets ..	"Ulysses"	1,041-06	" 9, "	" 12, "	764	142-54
32	35	Dances ..	"Scotus"	622-60	June 23, "	June 23, "	730	87-16
1605	176	Bars, iron ..	"Ulysses"	69-81	May 9, "	August 12, "	826	11-06
270	2	Soap ..	"Pyrrhus"	1,008-17	April 9, "	" 12, "	856	313-25
295	137	Tank plate ..	"Ulysses"	3,124-83	May 9, "	" 12, "	856	495-00
1605	1,342	Bars, iron ..	"Pyrrhus"	12-56	July 19, "	" 12, "	765	1-84
1605	1	Letter stamps ..	"Machon"	..	..	..	..	..
38	1	Total ..	..	..	..	..	..	2,837-77

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

(No. 2.)

Thames Iron Works.

To interest for three years, from 1st July, 1900, to 1st July, 1903, on their account 3,832l. 9s. 11d., at 3½ per cent. per annum, for materials landed at Taitienwan

£ s. d.
402 8 0

(Signed)

(Thames Iron Works.)

(No. 3.)

Fearon, Daniell and Co.

To interest on their account from 5th April, 1901, for 677 packages steel bars, angles, plates, and rivets stored at Hisho

£ s. d.
175 19 10

(No. 4.)

John Birch and Co. (Limited).

To interest on their account for 2,500l., for wheels and axles for tenders, stored in England, for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent. per annum

£ s. d.
262 10 0

(No. 5.)

Mackenzie and Co.

For boiler tubes and boiler covering for "Chingwo," on account of indent 1899, 599 packages

£ s. d.
756 13 4

(No. 6.)

The Imperial Chinese Railways of North China, debtor to the American Trading Company.

100 cases 16 x 12 window glass, at 4-85 taels

Taels.
485

485 taels =

£ s. d.
69 5 9For the American Trading Company,
(Signed) , Agent.

Tien-tsin, December 4, 1899.

Imperial Railways of North China, debtor to Buchheister and Co. (Limited), Tien-tsin.

	Dollars.	Cts.
--	----------	------

	Dollars.	Tael.
For the following, viz.:—		
Charges on railway material shipped from Shanghai to Tien-tsin per steam-ships "Ningpo" and "Chang Chow"—		
Charges in Shanghai, as per statement No. 1 .. .. .	..	26,391·06
" " No. 2 .. .. .	..	9,572·62
Charges in Tien-tsin—		
Taku Tug and Lighter Co. Voucher No. 10	3,050·25	..
" No. 11	522·90	..
Interest, 18th to 30th December (13 days) .. .. .	..	8·55
Telegrams and postages re "Ningpo"	75·90	..
" " "Chang Chow" .. .. .	..	15·10
Consulwh to Captain of "Ningpo," 150 Shanghai taels, at 101½ .. .. .	..	147·78
Commission, 1½ per cent.	3,657·75 54·87	36,026·56 540·39
	3,712·62	36,566·95
	£ s. d. 294 1 6	£ s. d. 4,056 12 11
Rate of exchange, 71·4 taels = 1 dollar at 2s. 2½d. = ..		<div style="border: 1px solid black; padding: 5px; width: fit-content; margin: auto;">£4,330 14s. 5d.</div>

(No. 10 (1).)

Tien-tsin, December 30, 1902.

Received payment :
(Signed) BÜCHHEISTER AND Co. (Limited).
December 31, 1902.

(No. 10 (1).)
Imperial Railways of North China, debtor to Buchheister and Co. (Limited).

Received payment :

Received payment :

Shanghai Tails.			
For the following, viz.:-			
Charges on Railway material forwarded per "Ningpo" to Hsinho Wharf:-			
Storage, Shanghai Hongkew Wharf Company, as per:-			
Voucher No. 1 ..	..	..	9,147 '60
" No. 2 ..	..	..	273 '90
" No. 3 ..	..	..	845 '90
" No. 4 ..	..	..	6,419 '40
Wharfage and night and Sunday time, as per Voucher No. 5 ..			
	..	..	626 '00
Suedvored, as per Voucher No. 6 ..			
	..	..	653 '90
350-47 dollars at 74'4125, as per Voucher No. 7 = ..			
	..	..	260 '79
Freight on account from 14th to 26th November, approximate arrival of steamer at Taku, 10,400 dollars, at 74'4125 ..			
	..	..	7,758 '90
Balance of freight, to be settled on completion of discharge (Voucher No. 8) ..			

Marine insurance, £30,000 = 266,666-66 taels at $\frac{1}{4}$ per cent. (Voucher No. 9) ..			
	..	..	666 '67
Shanghai telegrams, 52'48 dollars, at 74'4125 ..			
	..	..	59 '05
Bill brokerage, $\frac{1}{2}$ per cent. ..			26,652 '11
			33 '32
			26,685 '43
Tien-tsin Tails.			
At 101 $\frac{1}{2}$..			26,291 '06

Tien-tsin, December 30, 1902.

Received payment :
(Signed) BÜCHHEISTER AND Co. (Limited).

[1961 年—3]

E

(No. 11.)

IMPERIAL CHINESE RAILWAYS.

Summary of Claims for Indemnity, Engineering Departments.

	£
Schedule (A).—Rolling-stock	45,900
" (B).—Tien-tsin to Peking	144,380
" (C).—Tien-tsin to Kuyeh	7,480
" (D).—Outside Wall	61,333
" (E).—Telegraph	11,150
" (F).—Bridge works	12,928
	<u>313,171</u>

(Signed) C. W. KIRDER.

February 9, 1903.

NOTE.—This claim does not include—

- (1.) Compensation to staff.
- (2.) Destruction of head office, Tien-tsin.
- (3.) Cost of storage and reshipping materials from Europe and Shanghai.
- (4.) Interest payable on debts.

(No. 11 (1).)

SCHEDULE (A).

Indemnity Claim for Losses to Cars, Locomotives, and Shops.

	£
Valuation of rolling-stock on June 1900, see attached report ..	345,000
76 mud cars, Imperial Chinese Railway type	3,940
200 German ditto, ditto	8,000
	<u>355,940</u>
Revaluation of same in October 1902, see attached report ..	374,300
59 mud cars, Imperial Chinese Railway type	2,360
92 German ditto, ditto	3,580
	<u>380,340</u>
Increased value.	<u>33,800</u>

(No. 11 (2).)

SCHEDULE (A).—continued.

	£
Cost of repairing and rebuilding 500 cars used by Russians, at 30% each	15,000
Cost of repairing and rebuilding 8 locomotives destroyed by Boxers, P.K. line	9,600
Repairs and rebuilding 26 locomotives partly destroyed by Russians outside Wall	10,400
Loss of materials for car and locomotive building at Tongshan (see attached Schedule X)	44,700
	<u>79,700</u>
Claim amount, as above	79,700
Less—Increase in value of stock due to building of same during British Railway Administration	33,800
Balance due	<u>45,900</u>

(No. 11 (3).)

SCHEDULE (B).

Indemnity Claim for Losses and Damage to Tien-tsin-Peking Railway.

	£	£
80 miles of 2nd track—		
Rails, 12,000 tons, at 7L	84,000	
Sleepers	30,000	
Ballast	12,000	
Platelaying	3,000	
Points and crossings	1,000	
Track	<u>130,000</u>	130,000
Cost of renewals and repairs of girders, both tracks	2,700	
Floors of same, ditto, ditto	1,400	
Destruction of Fenghai shops	1,200	
Stores lost therein, ditto	4,500	
Plant, ditto, ditto	4,500	
Water cranes, pipes, &c.	350	
3 iron godowns and tracks	3,000	
Losses of sidings and crossings	1,000	
Locomotive stores	200	
Extra length of shed (engine)	250	
Repairs and renewals of stations	2,000	
M.C.P. station and yard	7,000	
Merchant hongs therein	4,000	
Main line signals	2,000	
Electric tramway and plant	20,000	
Main road and steam-roller	2,000	
	<u>186,100</u>	

(No. 11 (4).)

SCHEDULE (B).—continued.

Value of Works carried out by British Railway Administration for Capital Account, &c.

Chien-Men extension (value of rails included)	£
12 miles of rails left in stock	32,000
	9,720
	41,720
Claim, as per sheet 1	186,100
Less—Value of work done by British Railway Administration on extensions	41,720
	144,380

(No. 11 (5).)

SCHEDULE (C).

Indemnity Claim for Damages done Inside Wall between Tien-tsin and Kuyeh.

Mr. Tuckey's Report and Estimate	Dollars.
Extra new iron for roofs	193,658
	3,812
Mr. Marshall's Statement as to timber at Hsiao in 1900—	Dollars.
1900 value	181,200
1902	169,000
Loss	12,200
	12,200
Sale of 75-lb. rails to Hiling Railway	209,770
	120,000
Balance due	89,770
	£
At 12 dollars = 1L	7,480

(No. 11 (6).)

SCHEDULE (D).

Indemnity Claim for Losses incurred Outside Wall.

Damage as per Mr. Cox' Report and Estimate	Dollars.
Extra for Blake crusher	761,530
Rebuilding of defective piers of bridges	2,500
Sinking of turntable Y. K.	3,500
	1,000
	768,530
Estimated value of work done by Russians for capital account of railway	155,194
Balance due	613,336
	£
At 10 dollars = 1L	61,333

(No. 11 (7).)

SCHEDULE (E).

Indemnity Claim for Injury done to Telegraph System.

Injury done Inside Wall (see Mr. Huang's Report)	Taels.
Injury done Outside Wall	50,415
	38,905
Total	89,310
	£
At 8 taels = 1L	11,150

(No. 11 (8).)

SCHEDULE (F).

Indemnity Claim for Losses incurred at Shanhaikwan Bridge Works.

Loss of materials, tools, and damage to plant, as per Mr. Howard's Report and Estimate	£
	42,928

(No. 11 (9).)

SCHEDULE (X).

Approximate Value of Stock. Tongshan Workshops and Locomotive Sheds.

12th moon, 25th year of Kuang-Hsu (25th January, 1900) ..	Taels.
Materials approximately received from 25th January to 17th June, 1900, to meet the requirements for the erection of new rolling-stock—	977,044
Inside Wall	67,126
Outside Wall	283,605
Against running store indents	58,651
	1,386,426
Issues—February, March, April, and May, 1900, 65,000 taels per month	260,000
	1,126,426
Less—Value of stock found on the 1st October, 1903, 1,131,371 dollars at 70 ex.	791,900
	334,526
	£
At ex. 7.50 =	44,702*

[1961 2-3]

* See Schedule (A), Sheet 2.

F

(No. 12.)

	£
Advanced to British Railway Administration by British Government, for replacement and renewals caused by Boxer damage..	90,000

(No. 13.)

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account. Losses at Head Office, Tien-tsin.

	Taels.
Light rails and cars	14,000
Furniture, safes, &c.	10,000
Stationery, &c.	5,000
Dollars and fractional coins	18,000
	47,000
	£ 6,714
At 70 ex. =	

(No. 14.)

Miscellaneous Taxes.

	Dollars.	£ s. d.
1. Material for road bridge over Peiho, taken at 50 per cent. of Invoice No. 435 for 6354.2s.		317 12 0
2. Coal delivered to brick contractor at Kaopantze, the property of Railways. Total lost, 200 tons, at 6 dollars	1,200 00 ex. at 10 =	120 0 0
3. Kwo Hsing Chen's store account at Yang-tsun	357 39 "	35 14 6
4. Suen Ming Yu's claim for house at Chao Chia Tun, occupied by Mr. McKenzie, destroyed by Boxers. Estimated value	3,500 00 "	350 0 0
5. Honorarium to volunteer corps at Tong-shan	2,000 00 "	200 0 0
6. Pay of men working at car repairs in Feng-tai, 1st day, 8th moon, 5th year of Kuang-Hsu, 26.	157 88 "	15 15 0
7. Pay and passage money of Shan Singh, &c., station police	495 00 "	49 10 0
		1,088 11 6
Inside Wall		618 11 6
Outside Wall		470 0 0
		1,088 11 6

(No. 15.)

SCHEDULE (C).

Compensation to Staff which was retained in the East ready for immediate re-employment under British Railway Administration Service.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Barber, J.	Five months' half-pay from August to December, at 11l. per month = 55l. House allowance, nine months, at 150 dollars	550 00 1,350 00	1,900 00
Cox, A. G.	Was paid full salary. House allowance, nine months, at 150 dollars Less 350 dollars, drawn at Yingkow	1,350 00 350 00	1,000 00
Cheyne, J. J.	Five months' half-pay, at 10l. = 50l. House allowance, nine months, at 150 dollars	500 00 1,350 00	1,850 00
Engstran, H.	Three months at half-pay, at 13l. per month = 39l. Nine months' house allowance, at 150 dollars	390 00 1,350 00	1,740 00
Foley, J. E.	Received full pay. Nine months' house allowance, at 150 dollars	1,350 00	1,350 00
Harris, F. A.	Five months' half-pay, at 17l. 10s. per month = 87l. 10s. Nine months' house allowance, at 150 dollars	875 00 1,350 00	2,225 00
Howard, W. G.	Five months' pay, at 22l. 18s. 4d. = 114l. 11s. 8d. House allowance, nine months, at 150 dollars	1,145 82 1,350 00	2,495 82
Jamieson, F. A.	Five months' half-pay, at 20l. 16s. 8d. = 104l. 3s. 4d. House allowance, nine months, at 150 dollars	1,041 62 1,350 00	2,391 62
Kitching, F.	Two months at half-pay, at 11l. = 22l. House allowance, nine months, at 150 dollars	220 00 1,350 00	1,570 00
Kinder, C. W.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Leitch, W. O.	As per claim, 106l. 1s., at 10 dollars	1,060 50	1,060 50
Martin, J. C.	As per claim, 109l. 13s. 8d. And tael, 100 at 70	1,096 83 143 00	1,239 83
Marshall, H. J. W.	Five months' half-pay, at 20l. 16s. 8d. = 104l. 3s. 4d. House allowance, nine months, at 150 dollars Passage-money unpaid, 50l.	1,041 62 1,350 00 500 00	2,891 62
Moffat, J.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Moore, J.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Moorhead, H. B.	Five months' half-pay, at 8l. 6s. 8d. = 41l. 13s. 4d. Half passage, 25l.	416 62 250 00	666 62
Maclelland, J.	Three months' house allowance, at 150 dollars Went on leave home.	450 00	450 00

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Newmarch, L. J. ..	Five months' half-pay, at 25l. = 125l. House allowance, nine months, at 150 dollars	1,250-00 1,350-00	2,600-00
Rathnell, W. ..	Five months at half-pay, at 5l. 10s. = 27l. 10s. House allowance, nine months, at 150 dollars	275-00 1,350-00	1,625-00
Reekerby, J. ..	On full pay drawn from military. House allowance, nine months, at 150 dollars	1,350-00	1,350-00
Ricketts, D. P. ..	Paid by military. House allowance, two months, at 150 dollars..	300-00	300-00
Regby, E. H. ..	Received full pay. House allowance, nine months, at 150 dollars Deduct 350 dollars, drawn at Yingkow	1,350-00 350-00	1,000-00
Roberts, H. ..	Three months' full pay, at 120 dollars .. Two months' half-pay, at 60 dollars .. House allowance, nine months, at 90 dollars..	360-00 120-00 810-00	1,290-00
Sheriff, A. ..	Received full pay. No house allowance.		
Tuckey, T. W. T. ..	Fully paid.		
Wheeler, A. ..	Five months' half-pay, at 9l. 10s. = 47l. 10s. House allowance, nine months, at 150 dollars	475-00 1,350-00	1,825-00
Whitcombe ..	Full pay to October, paid by military. House allowance, nine months, at 150 dollars	1,350-00	1,350-00
Messrs. Irwin ..	Half-pay from 31st July, 1900, to 28th February 1901, to date of British Railway Administration handing over to Imperial Chinese Railway, 7 months at 20l. per month, 140l.	1,400-00	1,400-00
	Total		39,621-00

		Dollars.	£
Inside Wall		31,730-68 =	3,173
Outside Wall		7,890-33 =	789
			3,962

(No. 16.)

SCHEDULE (C).

Staff remaining in East, but not employed by Imperial Chinese Railway or British Railway Administration.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Brown, H. H. ..	Five months in lieu of notice, at 12l. 10s. = 62l. 10s. Six months' house allowance, at 150 dollars..	625-00 900-00	1,525-00
Dunn, R. C. A. ..	Five and a-half months in lieu of notice, at 16l. 13s. 4d. = 83l. 6s. 8d. Was not asked to remain.	833-33	833-33
Dixon, A. ..	Required by re-employed.		
Gibson, R. G. ..	Re-employed by British Railway Administra- tion. Five months' half-pay due in lieu of notice, at 16l. 13s. 4d. = 83l. 6s. 8d. .. Less, 350 dollars drawn at Yingkou ..	838-33 350-00	488-33
Griffith, D. P. ..	Five months' half-pay in lieu of notice, at 20l. 16s. 8d. = 104l. 3s. 4d.	1,041-62	1,041-62
Knowles, G. S. ..	Two months at half-pay, at 12l. 10s. = 25l.. Three months' full pay, at 25l. = 75l. ..	250-00 750-00	1,000-00
Krier, Otto ..	Three months' full pay, at 200 dollars ..	600-00	600-00
Norregard, B. W. ..	Five months' half-pay, at 18l. 15s. = 93l. 15s.	937-50	937-50
Palmer, R. J. ..	Five months' half-pay, at 9l. 5s. = 46l. 5s. ..	462-50	462-50
	Total		6,883-28

		Dollars.	£
Inside Wall		5,104-12 =	511
Outside Wall		1,779-16 =	178
		6,883-28 =	689

(No. 17.)

SCHEDULE (C).

Staff which left the East, not required to Remain.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Bone, T. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . per month = 90 <i>l</i> . ..	900-00	900-00
Clarke, C. ..	Five months' full pay, at 19 <i>l</i> . = 135 <i>l</i> . ..	1,350-00	1,350-00
Clarke, J. ..	Five months' full pay, at 15 <i>l</i> . = 275 <i>l</i> . ..	750-00	750-00
Currie, O. ..	Six months' pay } Seven months' leave } 650 <i>l</i> . ..	6,500-00	6,500-00
Dunn, E. C. A. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . = 90 <i>l</i> . ..	900-00	
	Deduct 350 dollars, drawn at Ying-kou ..	350-00	550-00
Durrell, H. ..	Five months' house allowance, at 18 <i>l</i> . = 90 <i>l</i> . ..	900-00	900-00
Emmerson, A. ..	Five months at half-pay, at 11 <i>l</i> . 10 <i>s</i> . per month = 57 <i>l</i> . 10 <i>s</i> . ..	575-00	
	Five months' house allowance, at 150 dollars ..	900-00	
	Less 350 dollars: was retained at Ying-kou for six months ..	1,475-00	
		350-00	1,125-00
Early, W. H. ..	Five months' half-pay, at 12 <i>l</i> . 10 <i>s</i> . = 62 <i>l</i> . 10 <i>s</i> . ..	625-00	625-00
Franklin, H. ..	Five months' full pay, at 18 <i>l</i> . 10 <i>s</i> . = 92 <i>l</i> . 10 <i>s</i> . ..	925-00	925-00
Gennill, J. ..	Five months' pay in lieu of notice, at 58 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 291 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . ..	2,916-62	
	Less, 350 dollars, drawn at Ying-kou ..	350-00	2,566-62
Hurst, F. ..	As per Claim, 631 <i>l</i> . 18 <i>s</i> . 4 <i>d</i> . ..	6,319-16	6,319-16
Hancock, G. W. ..	Five months' full pay in lieu of notice, at 22 <i>l</i> . = 110 <i>l</i> . ..	1,100-00	1,100-00
Hoyle, A. B. ..	Five months at half-pay, at 18 <i>l</i> . 15 <i>s</i> . = 93 <i>l</i> . 15 <i>s</i> . ..	937-50	937-50
Jackson, J. E. ..	Five months' full pay, at 33 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 166 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . ..	1,666-62	
	Less, 350 dollars, drawn at Ying-kou ..	350-00	1,316-62
Mackenzie, A. C. ..	Five months at half-pay, at 16 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . = 83 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . ..	833-33	
	Less, 350 dollars, paid at Ying-kou ..	350-00	483-33
Robke, O. ..	Five months' half-pay, at 9 <i>l</i> . = 45 <i>l</i> . ..	450-00	
	Less, 350 dollars, drawn at Ying-kou ..	350-00	100-00
Simmon, G. ..	Five months' half-pay at 12 <i>l</i> . 10 <i>s</i> . = 62 <i>l</i> . 10 <i>s</i> . ..	625-00	
	Less, 350 dollars, drawn at Ying-kou ..	350-00	275-00
Statham, E. E. ..	Five months' full pay, at 33 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 166 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . ..	1,666-66	1,666-66

Names.	Particulars.	Amount.	Total.
Plowman, W. ..	Five months' full pay, at 20 <i>l</i> . = 100 <i>l</i> . ..	Dollars. 1,000-00	Dollars.
Packham, R. ..	Was engaged, but never served on railway, and during trouble was paid passage. Say, two months' pay as compensation, at 18 <i>l</i> . = 36 <i>l</i> . ..	360-00	1,000-00
	Half-month's house allowance ..	75-00	
Stafford, W. R. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . 10 <i>s</i> . = 92 <i>l</i> . 10 <i>s</i> . ..	925-00	435-00
Strong, J. ..	Six months' pay in lieu of notice, decreased relative claim, at 25 <i>l</i> . = 150 <i>l</i> . ..	1,500-00	925-00
Symonds, T. ..	Five months' full pay in lieu of notice, at 26 <i>l</i> . = 130 <i>l</i> . ..	1,300-00	1,500-00
Terris, R. ..	Five months' full pay in lieu of notice, at 30 <i>l</i> . = 150 <i>l</i> . ..	1,500-00	1,300-00
Tumber, R. S. ..	Five months' half-pay, at 9 <i>l</i> . 5 <i>s</i> . = 23 <i>l</i> . 2 <i>s</i> . 6 <i>d</i> . ..	231-25	1,500-00
	Total ..		35,281-14

Inside Wall ..	Dollars.	£	s.	d.
Outside Wall ..	27,197-91 =	2,719	16	0
	8,083-23 =	808	6	6
	35,281-14 =	3,528	2	6

(No. 18.)

Wages and Compensation.

For wages and compensation to coolies accompanying Seymour expedition ..	Dollars.	£
	4,019-98	402

(No. 19.)

Contingent Fund.

For this amount set aside for contingent purposes ..	Tsels.	£
	50,000-00	7,500

(No. 20.)

British and Chinese Corporation.

British and Chinese Corporation claim for losses at Nansiao. (See Reference No. 1) ..	£	s.	d.
	8,980	1	5

Imperial Chinese Railways, debtor to British and Chinese Corporation (Limited).

	£	s.	d.
For amount of claim on account of losses in connection with the boring operations at Nanchiao as presented to His Britannic Majesty's Claims Commissioner	12,012	7	4
Plus claimed for damages, i.e., interest on money, expenses of general and head office, Administration charges, postponement of profit, &c., say, 15 per cent.	1,801	17	1
	13,814	4	5
Less amount of indemnity allowed by Claims Commissioner under the assumption that a portion of the plant remained uninjured	4,834	3	0
	8,980	1	5

This claim is based upon information of the total destruction of everything by native soldiers or brigands early in 1901 (see copies of correspondence attached).

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

(No. 21.)

This item of 4,000*l.* represents, according to Mr. Kinder's estimate, the cost of a resurvey of the railway lands.

This resurvey is made necessary by the loss or destruction of all the title-deeds which were kept in the safes at the head office in Tien-tsin. These deeds were the railway's sole evidence of title, as there seems to have been no official registration of the lands from time to time acquired by the railways.

The British and Chinese Corporation, as mortgagee, is interested in the re-establishment of the title for the purposes of the security, and this can be accomplished only upon the basis of a resurvey.

It is submitted, therefore, that the expense in question is a direct consequence of the Boxer uprising, and may properly be included in the indemnity claim.

AFFAIRS OF CHINA.

[May 23.]

CONFIDENTIAL.

SECTION 3.

[No. 1.]

Mr. Townley to the Marquess of Lansdowne.—(Received May 23.)

(No. 138.)

My Lord,

Peking, April 9, 1903.

I HAVE the honour to transmit herewith to your Lordship, in original, and with reference to your despatch No. 42 of the 13th February last, a detailed Statement of the Northern Railways claim as furnished to me by Mr. Cousins.

Your Lordship will perceive that the total claim for both intra- and extra-mural lines amounts to 448,836 *l.* 13*s.* 9*d.*, to which will have eventually to be added a sum of 4,000*l.*, the estimated cost of the resurvey of the line which will have to be made in order to establish the titles to lands of which the deeds were lost in the destruction of the head office at Tien-tsin. The above-mentioned sum, divided, I am told, as well as may be between the two lines, is made up of:—

	£	s.	d.
Intra-mural line	319,680	18	10
Extra-mural line	121,655	14	11
Contingent fund	7,500	0	0
Total	448,836	13	9

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his despatch No. 320 of the 29th October last, has caused him to place the Engineering Department's claim for damage to that line at 104,010*l.* Only a small portion of this last figure covers damage to rolling-stock, and that chiefly applicable to engines, since it was not possible to make a full separate allocation of rolling-stock to the one line or the other, and where separation has not been made the rolling-stock and supplies have been attributed to the intra-mural line.

Mr. Cousins tells me that it is possible that the sum of 300,000*l.* claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which, he is given to understand, amounts to about 150,000*l.*, although, as far as he can ascertain, no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to your Lordship at a later date.

From the appended Statement, it would, however, appear that the sum of 500,000*l.* claimed on account of the intra-mural line would prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr. Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its rolling-stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which, however, in the shape of bonds, is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to your Lordship upon its receipt.

Mr. Cousins points out that the sum of 30,000*l.* referred to in your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which your Lordship will see that the sum of 90,000*l.* advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr. Cousins and Mr. Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of 7,500*l.* seems to be a necessary precautionary measure to take, and if the money is not required it will not be used.

[1961 z—3]

B

References 15, 16, and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although, in most cases, the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon 9,000*l.* at which the amount of possible compensation has been fixed may be somewhat reduced. A Notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the Notice. It is clear that, without such public announcement, the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement of claims if the document is printed, as press of work has prevented a copy being made here.

I have, &c.

(Signed) WALTER TOWNLEY.

P.S.—Since writing the above, I have received an amended statement from Mr. Cousins, which includes the 4,000*l.* on account of resurvey, together with a Memorandum explaining the necessity for the same. In transmitting these documents to your Lordship, I have also the honour to inclose copy of a letter just received from Mr. Cousins, covering the railway claim, pointing out that the Railway Administration is seriously hampered by want of funds, and expressing a hope that His Majesty's Government will come to an early decision as to when, and how, the final claim allowed will be made available, and in what amounts.

W. T.

Inclosure 1 in No. 1.

Mr. R. Cousins to Mr. Townley.

Sir, REFERRING to the joint communication from Mr. Hewart and myself of the 27th April last, I now beg to hand you herewith, for the information of His Majesty's Government, a copy of the railway claim, as amended, amounting to 452,836*l.* 13*s.* 9*d.* The Administration holds, of course, the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by His Majesty's Government to examine and deal with same.

The Administration would, however, suggest the expediency of the examination being conducted in Tien-tsin, where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that, in consequence of the large expenditure already incurred in connection with the work of restoring the line to its anti-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of the 26th January, I venture to express the hope that His Majesty's Government will come to a decision as soon as possible when, and how, the final claim allowed will be made available, and in what amounts.

I understand that, owing to questions at issue between the Powers and Chinese Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of bonds representing the British share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted, and the settlement of the railway claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration, not only with regard to the claims of firms and individuals, for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that His Majesty's Government will see its way to arrange for the examination and settlement of the claim as soon as possible.

I have, &c.

(Signed) ED. COUSINS,

Representing British and Chinese Corporation (Limited).

Inclosure 2 in No. 1.

DETAILED STATEMENT OF THE NORTHERN RAILWAY CLAIM.

SUMMARY OF CLAIM.

No.		Inside Wall			Outside Wall			Total		
		£	s.	d.	£	s.	d.	£	s.	d.
1	Jardine, Matheson, and Co. As per statement of 22,057-07 bags, at 7 taels	..	..	..	..	..	..	..	..	..
2	Thames Ironworks. Interest for three years, from 1st July, 1900, to 1st July, 1903, on their account, 3,832 <i>l.</i> 11 <i>s.</i> 11 <i>d.</i>	..	..	..	..	..	..	..	..	..
3	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
4	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
5	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
6	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
7	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
8	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
9	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
10	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
11	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
12	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
13	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
14	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
15	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
16	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
17	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
18	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
19	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
20	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
21	For 34 per cent. per annum (for material landed at Tientsin)	..	..	..	..	..	..	..	..	..
	Total	..	..	..	..	..	..	..	..	..

(Signed)

H. J. W. MARSHALL,

Secretary, Imperial Railways of North China.

Tien-tsin, December 17, 1902.

Dear Sir,

Referring to our communication of the 1st ultimo, and documents attached, we now beg to hand you herewith an amended Statement of telegraphic expenditure, which please substitute for the one rendered.

Our claims are now summarized as follows:—

		Tsels.
Amounts due for material lost through the Boxer troubles in 1900, together with interest thereon, as per separate Statement	£ s. d.	20,344·80 1,712·90
Expenditure on telegrams, as per Statements attached	£ s. d.	1,951 16 5 and
British and Chinese Corporation's claim for losses at Nampiao, less amount allowed by His Britannic Majesty's Claims Commissioner	£ s. d.	8,980 1 5
		10,931 17 10 and 22,057·70

We are, &c.
(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

M. T. Liang, Esq., Director,
Imperial Chinese Railways.

SUMMARY of Amounts due by the Imperial Chinese Railways to the Undersigned, on account of Material imported under their Indents and lost in consequence of the Boxer troubles of 1900, also of Interest as per Statements attached.

A/c Indent	No.	Tsels.
"	32	4,457·31
"	38	694·17
"	1605 (part)	1,612·27
"	1605/1608	5,174·39
"	1610	338·58
"	1678	5,230·31
"	6192, 14s. 9d., at 2s. 4½d.	2,837·77
"	Various, delivered to British Railway Administration	20,344·80
Total		

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 32 (part), London Invoice No. 68, due April 10, 1900.

		£ s. d.	£ s. d.
5 cases split and cotter pins .. .. .	.. .. .	.. .. .	47 7 0
53 " iron rivets .. .. .	.. .. .	.. .. .	232 5 0
3 " brass sheets .. .. .	.. .. .	.. .. .	.. .. .
3 " copper sheets .. .. .	.. .. .	.. .. .	.. .. .
2 " copper tubes .. .. .	.. .. .	.. .. .	.. .. .
1 case copper and brass tubes .. .. .	.. .. .	.. .. .	379 12 1
Freight and bill of lading .. .. .	.. .. .	38 0 4	
Insurance on 779L, at 12s. 6d., and stamp .. .. .	.. .. .	4 18 3	
Bill stamp .. .. .	.. .. .	0 7 0	
			43 5 7
			702 9 6
At ex. 2s. 4½d. .. .. .	.. .. .	.. .. .	Tsels.
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. per annum .. .. .	.. .. .	.. .. .	5,941·71
			1,135·81
Less amounts rec. d from the British Railway Administration for portions of above material taken over by them, plus interest calculated thereon as per Memo. at foot .. .. .	.. .. .	.. .. .	7,076·52
Balance due by the Imperial Chinese Railways .. .. .	.. .. .	.. .. .	2,618·21
			4,457·31
MEMO.		Tsels.	Tsels.
1901.			
October. Amount received from British Railway Administration .. .. .	.. .. .	1,339·24	
Add—Interest from 1st November, 1901, to 31st December, 1902, = 14 months, and 7 per cent. .. .. .	.. .. .	109·37	
1902.			1,448·61
April. Amount received from British Railway Administration .. .. .	.. .. .	1,117·45	
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent. .. .. .	.. .. .	52·15	
			1,169·60
Total .. .. .	.. .. .	.. .. .	2,618·21

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin November 1, 1902.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indents Nos. 1605 and 1608 (part), London Invoice No. 87, due April 10, 1900.

	£ s. d.	£ s. d.
18 cases bolt ends .. .. .	.. .. .	96 10 0
16 " weighing machines .. .. .	.. .. .	97 6 3
57 bars angle steel .. .. .	.. .. .	27 19 1
50 cases galvanized iron .. .. .	.. .. .	401 5 2
40 kegs brown paint oak .. .. .	.. .. .	.. .. .
48 drums drying oil thinnings .. .. .	.. .. .	.. .. .
6 " turpentine .. .. .	.. .. .	105 0 8
1 case chalk .. .. .	.. .. .	2 3 0
1 " pump leathers .. .. .	.. .. .	.. .. .
1 " padlocks and snaps .. .. .	.. .. .	11 1 10
1 " sledge hammers .. .. .	.. .. .	37 17 2
13 bundles miners drill steel .. .. .	.. .. .	.. .. .
Freight and bill of lading	102 4 10	.. .. .
Insurances, &c.	6 12 9	108 17 7
		886 0 9
		Taels.
		7,494.24
At ex. 2s. 4½d. = .. .. .	.. .. .	1,450.06
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. .. .. .	.. .. .	8,924.30
Less amounts received from British Railway Administration for portions of above material taken over by them, and interest calculated thereon as per Memo. at foot .. .. .	.. .. .	3,749.91
Balance due by the Imperial Chinese Railways .. .. .	.. .. .	5,174.39
		Taels.
		383.63
		3,366.28
		3,749.91

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 1610, London Invoice No. 89, due April 10, 1900.

	£ s. d.	£ s. d.
2 cases drawing material	60 5 1
Freight and bill of lading	1 13 11
Insurance on 70L and stamp	0 9 0
Bill stamp	0 1 0
		2 3 11
		62 9 0
		Taels.
		528.21
At ex. 2s. 4½d. =	100.79
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent.	689.00
Less amount received from British Railway Administration, as per Memo. at foot	290.42
Balance due by the Imperial Chinese Railways	338.58
		Taels.
		277.47
		12.95
		290.42

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL Railways of North China to the Undersigned debtor.

	£ s. d.
For amount due on account of 60-lb. rails delivered ex "Forest-dale" and "Macedonia" at Sin-Ho 20th June, 1901, under Indent No. 1576, and contract of 21st September, 1899, viz. :-	
4,701 rails = 1,252 1 3 17	
Less sales .. 3,600 .. 696 8 2 9	
2,101 .. 555 13 1 9	
At 7½. 16s. 1½d. =	9,772 17 3
	5,435 16 6
	4,337 1 10*
Plus interest, at 7 per cent. per annum, as follows :-	
On 2,195L 4s. 4d. from 20th May, 1901, to 20th September, 1901 = 3 months	38 8 4
" 3,240L 11s. 1d. from 20th May, 1901, to 20th February, 1902 = 8 months	151 4 6
" 4,337L 1s. 10d. from 20th May, 1901, to 20th November, 1902 = 17 months	430 1 11
9,772L 17s. 3d.	619 14 9†
	4,956 16 7
	Taels.
	41,650.38

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, October 8, 1902.

* Paid in London.
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† Outstanding 619L 14s. 9d. = 5,230.31 taels.

D

STATEMENT, showing Interest due to the Undersigned on Railway Material taken over by the British Railway Administration.

Indent No.	Number of Packages.	Material.	Vessel.	Amount of Invoice.	Due Date.	When Paid.	Period.	Interest.
32	17	Rivets ..	"Ulysses"	Tschi. 1,385-7	May 9, 1900	October 31, 1901	Days.	Tschi. 81-81
1605	34	Bolt ends ..	"	1,397-88	April 10, "	" 31, "	510	142-97
1608	1	Sledge hammers ..	"	145-16	August 29, "	November 29, "	458	12-76
	3	Zinc plates ..	"Marburg"	178-94	June 14, "	March 11, 1902	636	21-83
	4	Vices ..	"Ulysses"	632-83	August 29, "	" 11, "	560	70-09
38	1	Wire springs ..	"Machau"	977-81	July 19, "	" 11, "	601	112-70
	24	Batchet braces ..	"	908-29	August 29, "	" 11, "	606	105-56
	2	Baiting ..	"Machau"	3,222-40	July 19, "	" 11, "	588	383-88
1618	217	Shovels, picks, &c.	"	1,065-30	" 19, "	" 11, "	601	122-79
1617	11	Weighting machines ..	"	375-11	" 19, "	" 11, "	601	43-24
	38	Crabblies ..	"Ulysses"	3,428-07	April 10, "	May 12, "	763	501-63
1605	192	Swamp cut ..	"Pyrrhus"	262-39	" 10, "	" 12, "	763	39-42
1608	2	Drawing material ..	"	1,084-00	" 10, "	" 12, "	763	188-75
1610	25	Rivets ..	"Ulysses"	1,811-00	" 10, "	" 12, "	763	182-54
32	8	Domes ..	"Pyrrhus"	69-81	May 9, "	August 12, "	826	313-25
1605	176	Soap ..	"Ulysses"	1,408-17	April 9, "	" 12, "	826	495-00
293	2	Tank plate ..	"Machau"	12-56	July 19, "	" 12, "	763	1-84
1605	137	Bars, iron ..	"	..	..	..	..	..
	1,342	Letter stamps ..	"	..	..	..	..	..
38	1	Total ..	"	..	..	..	..	2,837-77

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

(No. 2.)

Thames Iron Works.

To interest for three years, from 1st July, 1900, to 1st July, 1903, on their account 3,832l. 9s. 11d., at 3½ per cent. per annum, for materials landed at Taitienwan

£ s. d.
402 8 0

(Signed)

(Thames Iron Works.)

(No. 3.)

Fearon, Daniell and Co.

To interest on their account from 5th April, 1901, for 677 packages steel bars, angles, plates, and rivets stored at Hisho

£ s. d.
175 19 10

(No. 4.)

John Birch and Co. (Limited).

To interest on their account for 2,500l. for wheels and axles for tenders, stored in England, for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent. per annum

£ s. d.
262 10 0

(No. 5.)

Mackenzie and Co.

For boiler tubes and boiler covering ex "Chingwo," on account of indent 1899, 599 packages

£ s. d.
756 13 4

(No. 6.)

The Imperial Chinese Railways of North China, debtor to the American Trading Company.

100 cases 16 x 12 window glass, at 4-85 taels

Tschi.
485

485 taels =

£ s. d.
60 5 9

For the American Trading Company,
(Signed)

, Agent.

Tien-tsin, December 4, 1899.

(No. 7.)

Arnhold, Karberg, and Co. Contract, May 19, 1900.

	Dollars.
1,291 tons Japanese coal delivered alongside railway wharf, Yingkow, at 7-85 dollars per ton.	10,134-35
Interest, 15th June to 25th September, 1901, 102 days, at 6 per cent.	169-92
	10,304-27
	Taels.
At 72½	7,470-60
	£ s. d.
At 7 taels	1,067 4 7

(No. 8.)

Chin Shan Bank.

	Taels.
To three years' interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on 15,070 taels	3,164-70
	£
At 7 taels	482

(No. 9.)

Foo Fung Sheng Bank.

	Dollars.
To interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on 8,000 dollars	1,480
	£
At 10 dollars	148

(No. 10.)

Imperial Railways of North China, debtor to Buchheister and Co. (Limited), Tien-tsin.

	Dollars.	Taels.
For the following, viz. :—		
Charges on railway material shipped from Shanghai to Tien-tsin per steam ships "Ningpo" and "Chang Chow"—		
Charges in Shanghai, as per Statement No. 1		26,291-06
Charges in Tien-tsin—		9,572-62
Taku Tug and Lighter Co. Voucher No. 10	3,050-25	
Interest, 18th to 30th December (13 days)	522-90	
Telegrams and postages re "Ningpo"	8-55	
"Chang Chow"	75-00	
Consul to Captain of "Ningpo," 150 Shanghai taels, at 101½		15-10
		147-78
Commission, 1½ per cent.	3,657-75	36,026-56
	54-87	540-39
	3,712-62	36,566-95
	£ s. d.	£ s. d.
Rate of exchange, 71-4 taels = 1 dollar at 2s. 2½d. =	294 1 6	4,056 12 11
	£4,350 14s. 5d.	

Tien-tsin, December 30, 1902.

Received payment :

(Signed) BUCHHEISTER AND Co. (Limited).

December 31, 1902.

(No. 10 (1).)

Imperial Railways of North China, debtor to Buchheister and Co. (Limited).

	Shanghai Taels.
For the following, viz. :—	
Charges on railway material forwarded, per "Ningpo" to Hsiao Wharf—	
Storage, Shanghai Hongkew Wharf Company, as per—	
Voucher No. 1	9,147-60
" No. 2	273-99
" No. 3	845-90
" No. 4	6,419-40
Wharfage and night and Sunday time, as per Voucher No. 5	626-00
Stevedore, as per Voucher No. 6	633-90
350-47 dollars at 74-4125, as per Voucher No. 7 =	260-79
Freight on account from 14th to 26th November, approximate arrival of steamer at Taku, 10,400 dollars, at 74-4125	7,738-90
Balance of freight, to be settled on completion of discharge (Voucher No. 8)	
Marine insurance, £30,000 = 266,666-66 taels at ½ per cent. (Voucher No. 9)	666-67
Shanghai telegrams, 52-48 dollars, at 74-4125	39-05
Bill brokerage, ½ per cent.	26,652-11
	53-32
	26,685-43
	Tien-tsin Taels.
At 101½	26,291-06

Tien-tsin, December 30, 1902.

Received payment :

(Signed) BUCHHEISTER AND Co. (Limited).

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E

(No. 11.)

IMPERIAL CHINESE RAILWAYS.

Summary of Claims for Indemnity, Engineering Departments.

	£
Schedule (A).—Rolling-stock	45,900
" (B).—Tien-tsin to Peking	144,380
" (C).—Tien-tsin to Kuyeh	7,480
" (D).—Outside Wall	61,333
" (E).—Telegraph	11,150
" (F).—Bridge works	42,928
	<u>313,171</u>

(Signed) C. W. KIRDER.

February 9, 1903.

NOTE.—This claim does not include—

- (1.) Compensation to staff.
- (2.) Destruction of head office, Tien-tsin.
- (3.) Cost of storage and reshipping materials from Europe and Shanghai.
- (4.) Interest payable on debts.

(No. 11 (1).)

SCHEDULE (A).

Indemnity Claim for Losses to Cars, Locomotives, and Shops.

	£
Valuation of rolling-stock on June 1900, see attached report	345,000
76 mud cars, Imperial Chinese Railway type	3,040
200 German ditto, ditto	8,000
	<u>355,040</u>
Revaluation of same in October 1902, see attached report	374,300
59 mud cars, Imperial Chinese Railway type	2,360
92 German ditto, ditto	3,680
	<u>380,340</u>
Increased value	<u>83,500</u>

(No. 11 (2).)

SCHEDULE (A).—continued.

	£
Cost of repairing and rebuilding 500 cars used by Russians, at 30l. each	15,000
Cost of repairing and rebuilding 8 locomotives destroyed by Boxers, P.K. line	9,600
Repairs and rebuilding 26 locomotives partly destroyed by Russians outside Wall	10,400
Loss of materials for car and locomotive building at Tongshan (see attached Schedule X)	44,700
	<u>79,700</u>
Claim amount, as above	79,700
Less—Increase in value of stock due to building of same during British Railway Administration	33,800
Balance due	<u>45,900</u>

(No. 11 (3).)

SCHEDULE (B).

Indemnity Claim for Losses and Damage to Tien-tsin-Peking Railway.

	£	£
80 miles of 2nd track—		
Rails, 12,000 tons, at 71l.	84,000	
Sleepers	30,000	
Ballast	12,000	
Platelaying	3,000	
Points and crossings	1,000	
Track	<u>130,000</u>	
Cost of renewals and repairs of girders, both tracks		130,000
Floors of same, ditto, ditto		2,700
Destruction of Fenghai shops		1,400
Stores lost therein, ditto		1,200
Plant, ditto, ditto		4,500
Water cranes, pipes, &c.		350
3 iron godowns and tracks		3,000
Losses of sidings and crossings		1,000
Locomotive stores		200
Extra length of shed (engine)		250
Repairs and renewals of stations		2,000
M.C.P. station and yard		7,000
Merchant hongs therein		4,000
Main line signals		2,000
Electric tramway and plant		20,000
Main road and steam-roller		2,000
		<u>186,100</u>

(No. 11 (4).)

SCHEDULE (B).—continued.

Value of Works carried out by British Railway Administration for Capital Account, &c.

Chien-Men extension (value of rails included)	£
12 miles of rails left in stock	32,000
	9,720
	41,720
Claim, as per sheet 1	186,100
Less—Value of work done by British Railway Administration on extensions	41,720
	144,380

(No. 11 (5).)

SCHEDULE (C).

Indemnity Claim for Damages done Inside Wall between Tien-tsin and Kuyeh.

Mr. Tuckey's Report and Estimate	Dollars.
Extra new iron for roofs	193,658
	3,812
Mr. Marshall's Statement as to timber at Hisho in 1960—	
1900 value	Dollars.
1902	181,200
	169,000
Loss	12,200
	209,770
Sale of 75-lb. rails to Hiling Railway	120,000
Balance due	89,770
	£
At 12 dollars = 1L	7,480

(No. 11 (6).)

SCHEDULE (D).

Indemnity Claim for Losses incurred Outside Wall.

Damage as per Mr. Cox' Report and Estimate	Dollars.
Extra for Blake crusher	761,530
Rebuilding of defective piers of bridges	2,500
Sinking of turntable Y. K.	3,505
	1,000
	768,530
Estimated value of work done by Russians for capital account of railway	155,194
Balance due	613,336
	£
At 10 dollars = 1L	61,333

(No. 11 (7).)

SCHEDULE (E).

Indemnity Claim for Injury done to Telegraph System.

Injury done Inside Wall (see Mr. Huang's Report)	Taels.
Injury done Outside Wall	50,415
	38,905
Total	89,310
	£
At 8 taels = 1L	11,150

(No. 11 (8).)

SCHEDULE (F).

Indemnity Claim for Losses incurred at Shanhaikwan Bridge Works.

Loss of materials, tools, and damage to plant, as per Mr. Howard's Report and Estimate	£
	42,928

(No. 11 (9).)

SCHEDULE (X).

Approximate Value of Stock. Tongshan Workshops and Locomotive Sheds.

12th moon, 25th year of Kuang-Hsu (25th January, 1900) ..	Taels.
Materials approximately received from 25th January to 17th June, 1900, to meet the requirements for the erection of new rolling-stock—	977,044
Inside Wall	67,126
Outside Wall	283,605
Against running store indents	38,631
	1,386,426
Issues—February, March, April, and May, 1900, 65,000 taels per month	260,000
	1,126,426
Less—Value of stock found on the 1st October, 1903, 1,131,371 dollars at 70 ex.	791,960
	334,566
	£
At ex. 7.50 =	44,702*

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* See Schedule (A), Sheet 2.

F

(No. 12.)

	£
Advanced to British Railway Administration by British Government, for replacement and renewals caused by Boxer damage.	90,000

(No. 13.)

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account. Losses at Head Office, Tien-tsin.

	Taels.
Light rails and cars	14,000
Furniture, safes, &c.	10,000
Stationery, &c.	5,000
Dollars and fractional coins	18,000
	47,000
	£
At 70 ex. =	6,714

(No. 14.)

Miscellaneous Taxes.

	Dollars.	£ s. d.
1. Material for road bridge over Peiho, taken at 50 per cent. of Invoice No. 433 for 6352 2s.		317 12 0
2. Coal delivered to brick contractor at Kaopantze, the property of Railways. Total lost, 200 tons, at 6 dollars	1,200 00 ex. at 10 =	120 0 0
3. Kwo Hseng Chen's store account at Yang-tsun	357 39 "	35 14 6
4. Suen Ming Yu's claim for house at Chao Chia Tun, occupied by Mr. McKenzie, destroyed by Boxers. Estimated value	3,500 00 "	350 0 0
5. Honorarium to volunteer corps at Tong-shan	2,000 00 "	200 0 0
6. Pay of men working at car repairs in Feng-tai, 1st day, 8th moon, 5th year of Kuang-Hsu, 26.	157 88 "	15 15 0
7. Pay and passage money of Shan Singh, &c., station police	495 00 "	49 10 0
		1,088 11 6
Inside Wall		618 11 6
Outside Wall		470 0 0
		1,088 11 6

(No. 15.)

SCHEDULE (C).

Compensation to Staff which was retained in the East ready for immediate re-employment under British Railway Administration Service.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Barber, J.	Five months' half-pay from August to December, at 1 1/2 per month = 55L House allowance, nine months, at 150 dollars	550 00 1,350 00	
Cox, A. G.	Was paid full salary. House allowance, nine months, at 150 dollars Less 350 dollars, drawn at Yingkow	1,350 00 350 00	1,900 00
Cheyne, J. J.	Five months' half-pay, at 10L = 50L House allowance, nine months, at 150 dollars	500 00 1,350 00	1,850 00
Engstron, H.	Three months at half-pay, at 13L per month = 39L Nine months' house allowance, at 150 dollars	390 00 1,350 00	1,740 00
Foley, J. E.	Received full pay. Nine months' house allowance, at 150 dollars	1,350 00	1,350 00
Harris, F. A.	Five months' half-pay, at 17L 10s. per month = 87L 10s. Nine months' house allowance, at 150 dollars	875 00 1,350 00	2,225 00
Howard, W. G.	Five months' pay, at 22L 18s. 4d. = 114L 11s. 8d. House allowance, nine months, at 150 dollars	1,145 82 1,350 00	2,495 82
Jamieson, F. A.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. House allowance, nine months, at 150 dollars	1,041 62 1,350 00	2,391 62
Kitching, F.	Two months at half-pay, at 11L = 22L House allowance, nine months, at 150 dollars	220 00 1,350 00	1,570 00
Kinder, C. W.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Leitch, W. O.	As per claim, 106L 1s., at 10 dollars	1,060 50	1,060 50
Martin, J. C.	As per claim, 109L 13s. 8d. And tael, 100 at 70	1,096 83 143 00	1,239 83
Marshall, H. J. W.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. House allowance, nine months, at 150 dollars Passage-money unpaid, 50L	1,041 62 1,350 00 500 00	2,891 62
Moffat, J.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Moore, J.	House allowance, nine months, at 150 dollars	1,350 00	1,350 00
Moorhead, H. B.	Five months' half-pay, at 8L 6s. 8d. = 41L 13s. 4d. Half passage, 25L	416 62 250 00	666 62
Maclelland, J.	Three months' house allowance, at 150 dollars Went on leave home.	450 00	450 00

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Newmarch, L. J. ..	Five months' half-pay, at 25l. = 125l. House allowance, nine months, at 150 dollars	1,250-00 1,350-00	2,600-00
Rathnell, W. ..	Five months at half-pay, at 5l. 10s. = 27l. 10s. House allowance, nine months, at 150 dollars	275-00 1,350-00	1,625-00
Reckerby, J. ..	On full pay drawn from military. House allowance, nine months, at 150 dollars	1,350-00	1,350-00
Ricketts, D. P. ..	Paid by military. House allowance, two months, at 150 dollars..	300-00	300-00
Regby, E. H. ..	Received full pay. House allowance, nine months, at 150 dollars Deduct 350 dollars, drawn at Yingkow	1,350-00 350-00	1,000-00
Roberts, H. ..	Three months' full pay, at 120 dollars .. Two months' half-pay, at 60 dollars .. House allowance, nine months, at 90 dollars..	360-00 120-00 810-00	1,290-00
Sheriff, A. ..	Received full pay. No house allowance.		
Tuckey, T. W. T. ..	Fully paid.		
Wheeler, A. ..	Five months' half-pay, at 9l. 10s. = 47l. 10s. House allowance, nine months, at 150 dollars	475-00 1,350-00	1,825-00
Whitecombe ..	Full pay to October, paid by military. House allowance, nine months, at 150 dollars	1,350-00	1,350-00
Messrs. Irwin ..	Half-pay from 31st July, 1900, to 28th February 1901, to date of British Railway Administration handing over to Imperial Chinese Railway, 7 months at 20l. per month, 140l.	1,400-00	1,400-00
	Total		39,621-00

		Dollars.	£
Inside Wall		31,730-68 =	3,173
Outside Wall		7,890-33 =	789
			3,962

(No. 16.)

SCHEDULE (C).

Staff remaining in East, but not employed by Imperial Chinese Railway or British Railway Administration.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Brown, H. H. ..	Five months in lieu of notice, at 12l. 10s. = 62l. 10s. Six months' house allowance, at 150 dollars..	625-00 900-00	1,525-00
Dunn, E. C. A. ..	Five and a-half months in lieu of notice, at 16l. 13s. 4d. = 83l. 6s. 8d. Was not asked to remain.	833-33	833-33
Dixon, A. ..	Required by re-employed.		
Gibson, R. G. ..	Re-employed by British Railway Administra- tion. Five months' half-pay due in lieu of notice, at 16l. 13s. 4d. = 83l. 6s. 8d. .. Less, 350 dollars drawn at Yingkou ..	838-53 350-00	483-33
Griffith, D. P. ..	Five months' half-pay in lieu of notice, at 20l. 16s. 8d. = 104l. 3s. 4d.	1,041-62	1,041-62
Knowles, G. S. ..	Two months at half-pay, at 12l. 10s. = 25l.. Three months' full pay, at 25l. = 75l. ..	250-00 750-00	1,000-00
Krier, Otto ..	Three months' full pay, at 200 dollars ..	600-00	600-00
Norregard, B. W... ..	Five months' half-pay, at 18l. 15s. = 93l. 15s.	937-50	937-50
Palmer, R. J. ..	Five months' half-pay, at 9l. 5s. = 46l. 5s. ..	462-50	462-50
	Total		6,883-28

		Dollars.	£
Inside Wall		5,104-12 =	511
Outside Wall		1,779-16 =	178
		6,883-28 =	689

(No. 17.)

SCHEDULE (C).

Staff which left the East, not required to Remain.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Bone, T. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . per month = 90 <i>l</i> . ..	900-00	900-00
Clarke, C. ..	Five months' full pay, at 19 <i>l</i> . = 135 <i>l</i> . ..	1,350-00	1,350-00
Clarke, J. ..	Five months' full pay, at 15 <i>l</i> . = 275 <i>l</i> . ..	750-00	750-00
Currie, O. ..	Six months' pay } 650 <i>l</i> . .. Seven months' leave }	6,500-00	6,500-00
Dunn, E. C. A. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . = 90 <i>l</i> . .. Deduct 350 dollars, drawn at Ying-kou ..	900-00 350-00	550-00
Dorrell, H. ..	Five months' house allowance, at 18 <i>l</i> . = 90 <i>l</i> . ..	900-00	900-00
Emmerson, A. ..	Five months at half-pay, at 11 <i>l</i> . 10 <i>s</i> . per month = 57 <i>l</i> . 10 <i>s</i> . .. Five months' house allowance, at 150 dollars ..	575-00 900-00	1,475-00
	Less 350 dollars; was retained at Ying-kou for six months ..	350-00	1,125-00
Early, W. H. ..	Five months' half-pay, at 12 <i>l</i> . 10 <i>s</i> . = 62 <i>l</i> . 10 <i>s</i> . ..	625-00	625-00
Franklin, H. ..	Five months' full pay, at 18 <i>l</i> . 10 <i>s</i> . = 92 <i>l</i> . 10 <i>s</i> . ..	925-00	925-00
Gennill, J. ..	Five months' pay in lieu of notice, at 58 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 291 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . .. Less, 350 dollars, drawn at Ying-kou ..	2,916-62 350-00	2,566-62
Hurst, F. ..	As per Claim, 631 <i>l</i> . 18 <i>s</i> . 4 <i>d</i> . ..	6,319-16	6,319-16
Hancock, G. W. ..	Five months' full pay in lieu of notice, at 22 <i>l</i> . = 110 <i>l</i> . ..	1,100-00	1,100-00
Hoyle, A. B. ..	Five months at half-pay, at 18 <i>l</i> . 15 <i>s</i> . = 93 <i>l</i> . 15 <i>s</i> . ..	937-50	937-50
Jackson, J. E. ..	Five months' full pay, at 33 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 1,666-62 .. Less, 350 dollars, drawn at Ying-kou ..	1,666-62 350-00	1,316-62
Mackenzie, A. C. ..	Five months at half-pay, at 16 <i>l</i> . 13 <i>s</i> . 4 <i>d</i> . = 833-33 .. Less, 350 dollars, paid at Ying-kou ..	833-33 350-00	483-33
Robke, O. ..	Five months' half-pay, at 9 <i>l</i> . = 45 <i>l</i> . .. Less, 350 dollars, drawn at Ying-kou ..	450-00 350-00	100-00
Simmon, G. ..	Five months' half-pay at 12 <i>l</i> . 10 <i>s</i> . = 62 <i>l</i> . 10 <i>s</i> . .. Less, 350 dollars, drawn at Ying-kou ..	625-00 350-00	275-00
Statham, K. E. ..	Five months' full pay, at 33 <i>l</i> . 6 <i>s</i> . 8 <i>d</i> . = 1,666-66 ..	1,666-66	1,666-66

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Plowman, W. ..	Five months' full pay, at 20 <i>l</i> . = 100 <i>l</i> . ..	1,000-00	1,000-00
Packham, R. ..	Was engaged, but never served on railway, and during trouble was paid passage. Say, two months' pay as compensation, at 18 <i>l</i> . = 36 <i>l</i> . .. Half-month's house allowance ..	360-00 75-00	435-00
Stafford, W. R. ..	Five months' full pay in lieu of notice, at 18 <i>l</i> . 10 <i>s</i> . = 92 <i>l</i> . 10 <i>s</i> . ..	925-00	925-00
Strong, J. ..	Six months' pay in lieu of notice, decreased relative claim, at 25 <i>l</i> . = 150 <i>l</i> . ..	1,500-00	1,500-00
Symonds, T. ..	Five months' full pay in lieu of notice, at 26 <i>l</i> . = 130 <i>l</i> . ..	1,300-00	1,300-00
Terrie, R. ..	Five months' full pay in lieu of notice, at 30 <i>l</i> . = 150 <i>l</i> . ..	1,500-00	1,500-00
Tumber, R. S. ..	Five months' half-pay, at 9 <i>l</i> . 5 <i>s</i> . = 23 <i>l</i> . 2 <i>s</i> . 6 <i>d</i> . ..	231-25	231-25
	Total ..	..	35,281-14

	Dollars.	£	s.	d.
Inside Wall ..	27,197-91 =	2,719	16	0
Outside Wall ..	8,083-23 =	808	6	6
	35,281-14 =	3,528	2	6

(No. 18.)

Wages and Compensation.

	Dollars.	£
For wages and compensation to coolies accompanying Seymour expedition ..	4,019-98	402

(No. 19.)

Contingent Fund.

	Tael.	£
For this amount set aside for contingent purposes ..	50,000-00	7,500

(No. 20.)

British and Chinese Corporation.

	£	s.	d.
British and Chinese Corporation claim for losses at Nampiao. (See Reference No. 1) ..	8,989	1	5

DRAFT OF PROPOSED SCHEME FOR FINANCING THE CHINESE

NORTHERN RAILWAYS INDEMNITY.

	£	s.	d.
For amount of claim on account of losses in connection with the boring operations at Nanpiao as presented to His Britannic Majesty's Claims Commissioner...	12,012	7	4
Plus claimed for damages, i.e., interest on money, expenses of general and head office, Administration charges, postponement of profit, &c., say, 15 per cent. ...	1,801	17	1
	13,814	4	5
Less amount of indemnity allowed by Claims Commissioner under the assumption that a portion of the plant remained uninjured	4,834	3	0
	8,980	1	5

This claim is based upon information of the total destruction of everything by native soldiers or brigands early in 1901 (see copies of correspondence attached).

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

(No. 21.)

This item of 4,000*l.* represents, according to Mr. Kinder's estimate, the cost of a resurvey of the railway lands.

This resurvey is made necessary by the loss or destruction of all the title-deeds which were kept in the safes at the head office in Tien-tsin. These deeds were the railway's sole evidence of title, as there seems to have been no official registration of the lands from time to time acquired by the railways.

The British and Chinese Corporation, as mortgagee, is interested in the re-establishment of the title for the purposes of the security, and this can be accomplished only upon the basis of a resurvey.

It is submitted, therefore, that the expense in question is a direct consequence of the Boxer uprising, and may properly be included in the indemnity claim.

For the purposes of this Draft Proposal it has been assumed that the Northern Railways Indemnity has been allowed at £450,000, which sum includes the claim of H.M.G. £90,000.

1. The sum of £800,000 of the British General Indemnity originally set apart to cover the Northern Railways Indemnity Claim shall be retained by the British Government as security for a loan to be raised to provide in cash the amount of the claim allowed, namely £450,000. The bonds representing the sum of £800,000 above mentioned are hereinafter referred to as Indemnity Security Bonds.

2. Only the yearly interest, namely £32,000 accruing on the Indemnity Security Bonds will be applied to the interest and amortization of the proposed loan. On the extinction of that loan the Indemnity Security Bonds will be free of all charge on that account, and will then be at the discretionary disposal of H.M.G. and H.M.G. if it so desires may arrange beforehand to return them to the Chinese Govt. for cancellation, in which case the said Indemnity Security Bonds need not participate in

the

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Peking April 9.

M^r Barclay.

No 138.

(Ref. F.O. No. 42, Feb. 13)

Recd 23. May 1903.

2. Includes

Northern Railways Indemnity Claim: To: detailed statement

Tracked for purchase.

the signature must

with M^r Barclay.

No 159.

Apr 22.

3. H.M.G. will not by any act of its own allow the provisions of Art.6 of the Protocol of 7 Sept,1901, so far as the same are applicable to the Indemnity Security Bonds, to be modified to their prejudice as such security.

4. The interest on the Indemnity Security Bonds as it accrues shall be handed to the Hongkong & Shanghai Banking Corpn. in London for the credit of the B. and C. Corpn. in trust, to be applied to the service of the proposed loan.

5. The proposed loan shall be evidenced by bonds of the Imp. Railways of N.China for the aggregate sum of £450,000, bearing interest at five per cent per annum, and shall be further secured by a second mortgage on the properties, rents and profits of the Imp. Railways of N.China, subordinated however to the existing first mortgage on the same.

6. It is believed that a loan thus doubly secured may be placed on the market at a price which would yield about eighty eight per cent net. The difference between this price and the par value of the bonds, namely twelve per cent, would be approximately covered by the interest which at the date of the issue of the new bonds would have already accrued on the

Indemnity Security Bonds. By the 30 June, 1903, the interest accrued on the Indemnity Security Bonds will amount to £48,000 and this is not including £16,000, interest due on 31 December 1901, payment of which is deferred in terms of the Protocol.

Stated differently, after applying the accrued interest, say £48,000 to payment on account of the claim allowed, it is proposed to raise the balance of said claim, namely £402,000 by a loan of a sum nominal sufficient to yield that amount net, or say, in round figures a loan of £450,000,

7. The interest on the Indemnity Security Bonds, namely £32,000 per annum, applied to the service of the second mortgage bonds will on a sinking fund basis extinguish the latter in about twenty five years; that is to say, by means of a half-yearly payment of 3.52581 % which, calculated on £450,000 is equal to £15,866, or a total yearly payment of £31,732. The small surplus of say £268 may be applied to defray expenses of the management of the Loan Service.

8. The second mortgage and second mortgage bonds shall contain, among other conditions, a guarantee by the Imp. Railways of N. China to make good any deficiency that may arise in the funds provided as above for the service of the proposed loan.

9.

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9. The proceeds of the proposed loan shall be paid to the Hongkong & Shanghai Banking Corpn. at London for the credit of the British and Chinese Corpn. in trust, to be applied to the purposes of the Northern Rys. Indemnity, and disbursements of the same shall be made only on the joint warrant of the representative of the British & Chinese Corpn. on the Railway Directorate in Tientsin, and of the Chief Engineer of the Imp. Railways of N. China, whose appointment is provided for by the first mortgage agreement.

(Sd). E.G. Hillier.

Peking. 21 April, 1903.

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Peking.

22 April, 1903.

No 159.

My Lord,

I had the honour to inform Your Lordship in my despatch No 138 of the 9th inst. forwarding a statement of the Northern Railways indemnity claim, that I learned from Mr Cousins, the representative at Tientsin of the British Bondholders, that the railway was in urgent need of funds to carry out necessary repairs, and that it was hoped that His Majesty's Government would come to an early decision as to the method in which the claim should be met.

I would now beg to submit to Your Lordship a scheme for the payment of the Railway Indemnity claim which has been drawn up by Mr E.G. Hillier, the Manager of the Peking branch of the Hongkong and Shanghai Bank, at the request of Mr Cousins.

Mr Hillier

The Marquess of Lansdowne K.G.

etc. etc. etc.

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Sub. in His Majesty's
No 159. Apr. 22/03.

Mr Hillier points out in the preamble to his scheme that the Railway claim was excluded from the method adopted for the payment of other private claims at the special instruction of His Majesty's Government, which reserved it for later consideration, and that if the British and Chinese Corporation was not in a position to offer a definite estimate of the damage done at the time of the adoption of the general scheme for the payment of British private claims, it was due to natural circumstances, and through no fault of their own.

Mr Hillier further demonstrates that, as far as his information goes, whereas other private claimants were given their choice of an early payment without interest or Chinese Government 4% bonds, the British and Chinese Corporation were given no option on account of the Railway claim, but were allotted bonds, which under any circumstances would be hard to place on the market, but which under the peculiar conditions prevailing may be considered as unmarketable. He also shows that about £30,000 on account of private claims have been included in the Northern Railway indemnity claim by the Claims Commissioner; that if the claimants are made to accept bonds

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bonds on account of their claims, they will be hardly treated as compared with other British private claimants, whereas if they are to be paid in cash the Railway Administration will be a heavy sufferer.

Mr Hillier has based his scheme upon the assumption that His Majesty's Government may feel itself in a position to retain the whole of the £800,000 set aside to cover the Railways indemnity claim, and he expresses a hope that should His Majesty's Government feel bound to return to the Chinese Government any portion of the above mentioned sum, such portion should not be more than sufficient to cover the Russian claim on account of the extra-mural line, and then only in such amount as would represent the total expenditure of the Russian military authorities on capital account, after deducting net earnings during their administration of the line. A full statement of the negotiations connected with the Russian railway claim will form the subject of an accompanying despatch.

For the purposes of his scheme, Mr Hillier has taken the sum of £450,000, the total amount of the claim

submitted

4- 2020 64

submitted to Your Lordship in my despatch No 138 of the 9th inst. on the hypothesis that such claim has been allowed, and he suggests that the £800,000 originally set apart to cover the Railway claim should be retained by His Majesty's Government as security for a loan to raise the cash amount of the claim allowed, hypothetically, and that the yearly interest on the Indemnity bonds, namely £32,000, be applied to the interest and amortization of the proposed loan. Mr Hillier calculates that with the sum of £48,000 which will have accrued as interest on the Railway Indemnity Bonds by the 30th of June next, the total amount of the loan will have been paid off in about 25 years, and that His Majesty's Government will be free to do what it judges suitable with the £800,000 of bonds earmarked for the Railway claim. Mr Hillier calls attention, in the concluding paragraph of his preamble to the fact that by his scheme the £800,000 of Indemnity bonds on the Railway's account will be excluded from the drawings for redemption of the British Indemnity, while the Chinese Government will benefit by the eventual cancellation of the principal indebtedness represented by them.

In

In submitting the scheme to Your Lordship's consideration, I would beg to remark that £450,000 of Chinese Indemnity bonds would in no way represent that sum in actual fact, and that should the Railway claim be liquidated by an allotment of the mere sum claimed in bonds, the compensation granted, in that it will not be negotiable, will fall far short of the ^{cost of the} damage done, whilst the Administration will be seriously hampered in the execution of necessary repairs and works by its inability to raise the funds immediately required for the carrying out of the same.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Lowndes

Mr. Towley No. 138 April 9, 03. }
 " No. 159 April 22, 03. } 169

We have had to wait until No. 138 could be
 got into print. The papers contain:

A. The detailed claim of the Northern
 Railways for the losses incurred inside and
 outside the wall.

B. Mr. Millin's scheme for the payment
 of the claim.

A. The total claim is £452,836.13.9
 but this does not include the claim of the
 Russian Railway Administration which has
 not yet been submitted but which is believed
 to amount to £150,000.

of

1905. Apr. 22.

Revd. Mr. Towley

(sup. in No. 138 of Apr. 9/03)

recd. June 8, 1905. Inds.

by leg.

Northern Railways

claim.

to Mr. Millin's scheme

for payment of: -

(Pam. (M. H.))

see minute with Mr.

Towley's No. 138.

498-1484

Of the Headings of the Claim (see page 3). the following seem to me to require consideration.

1. As to which on page 4: an item of £1951.16.5 for telegrams which is not explained.

11. and 12. pages 14 to 18. It is not clear whether the figures under 11. show the gross amount spent to repair the damage done to the line or what was spent in addition to the £90,000 advanced by the British Railway Administration. If they represent the former the £90,000 in 12. is a double charge.

20. This item is as debted to the B. and C Corporation "for amount of claim on account of

loss

losses in connection with the mining operations at Ranspie as presented to H. B. M. Claims Comm."

The claim is for £13,814.4.5 less £4,834.3.0 allowed by Claims Comm." under the assumption that a portion of the plant remained unimpaired.

If the claim has already been before the Claims Comm." we should ascertain why it is brought forward again before it is allowed.

It is possible that the B. and C. Corporation may be able to explain these three items. If not we must get the explanations from Mr. Torsley.

B. We must consult the Treasury about Mr. Hillier's Scheme. There is a good deal of force in Mr. Hillier's arguments when he compares the

treatment

treatment of the Railway Claim as compared with other
Private Claims. It may be mentioned that Mr. Hollier
and Mr. Brier have treated the R.R. Claim, although
it forms part of the Govt. claim, as a private
claim in so far that they have set aside
out of each half yearly payment money for
the payment of the interest on bonds for £800,000
for the R.R. This appears to be fair as the claim
is really private. It is known to be stated
that although Mr. Hollier anticipates the payment
out of the £800,000 of the claim of the Russian
Railway Administration he leaves that claim
out of account in his financial calculations.
~~I conclude that we shall eventually have to pay~~
~~the amount of the claim in Tientsin.~~

Q. A. Write to the B. & C. Corporation asking
whether they can give explanations on the three points
mentioned

Mentioned under A and send the
papers to the Treasury for opinion as to B.
telling them what we are doing about the
accounts, & mentioning point as to Russian R.R. Claim.

When we have got the explanations if we
want a further opinion about the figures we
can get the War Office to submit them to the
Officer who was in charge of the British R.R.
Administration in China - if he is available.

We shall also have to check the figures
and - I conclude - have the vouchers
examined at Tientsin.

H.R.

It seems to me very unlikely that the
£90,000 advanced by Kulgo has been charged
twice over, but we can ask. A more serious
question, I think, is how much of the £120,000 was
charged

AFFAIRS OF CHINA.

[June 8.]

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CONFIDENTIAL.

SECTION 5.

No. 1.

Mr. Townley to the Marquess of Lansdowne.—(Received June 8.)

(No. 159.)

My Lord,

Peking, April 22, 1903.

I HAD the honour to inform our Lordship in my despatch No. 138 of the 9th instant, forwarding a statement of the Northern Railways Indemnity Claim, that I learned from Mr. Cousins, the representative at Tien-tsin of the British Bondholders, that the Railway was in urgent need of funds to carry out necessary repairs, and that it was hoped that His Majesty's Government would come to an early decision as to the method in which the claim should be met.

I would now beg to submit to your Lordship a scheme for the payment of the Railway Indemnity Claim, which has been drawn up by Mr. E. G. Hillier, the manager of the Peking branch of the Hong Kong and Shanghai Bank, at the request of Mr. Cousins.

Mr. Hillier points out in the preamble to his scheme that the Railway Claim was excluded from the method adopted for the payment of other private claims at the special instruction of His Majesty's Government, which reserved it for later consideration, and that if the British and Chinese Corporation was not in a position to offer a definite estimate of the damage done at the time of the adoption of the general scheme for the payment of British private claims, it was due to natural circumstances, and through no fault of their own.

Mr. Hillier further demonstrates that, as far as his information goes, whereas other private claimants were given their choice of an early payment without interest or Chinese Government 4 per cent. bonds, the British and Chinese Corporation were given no option on account of the Railway Claim, but were allotted bonds, which under any circumstances would be hard to place on the market, but which under the peculiar conditions prevailing may be considered as unmarketable. He also shows that about 30,000*l.* on account of private claims have been included in the Northern Railway Indemnity Claim by the Claims Commissioner; that if the claimants are made to accept bonds on account of their claims, they will be hardly treated as compared with other British private claimants, whereas if they are to be paid in cash the Railway Administration will be a heavy sufferer.

Mr. Hillier has based his scheme upon the assumption that His Majesty's Government may feel itself in a position to retain the whole of the 800,000*l.* set aside to cover the Railways Indemnity Claim, and he expresses a hope that should His Majesty's Government feel bound to return to the Chinese Government any portion of the above-mentioned sum, such portion should not be more than sufficient to cover the Russian claim on account of the extra-mural line, and then only in such amount as would represent the total expenditure of the Russian military authorities on capital account, after deducting net earnings during their administration of the line. A full statement of the negotiations connected with the Russian Railway Claim will form the subject of an accompanying despatch.

For the purposes of his scheme Mr. Hillier has taken the sum of 450,000*l.*, the total amount of the claim submitted to your Lordship in my despatch No. 138 of the 9th instant, on the hypothesis that such claim has been allowed, and he suggests that the 800,000*l.* originally set apart to cover the Railway Claim should be retained by His Majesty's Government as security for a loan to raise the cash amount of the claim allowed, hypothetically, and that the yearly interest on the Indemnity Bonds, namely, 32,000*l.*, be applied to the interest and amortization of the proposed loan. Mr. Hillier calculates that with the sum of 48,000*l.*, which will have accrued as interest on the Railway Indemnity Bonds by the 30th June next, the total amount of the loan will have been paid off in about twenty-five years, and that His Majesty's Government will be free to do what it judges suitable with the 800,000*l.* of bonds earmarked for the Railway Claim. Mr. Hillier calls attention, in the concluding paragraph of his preamble, to the fact that by his scheme the 800,000*l.* of Indemnity Bonds on the Railways account will be excluded from the drawings for redemption of the British Indemnity, while the Chinese Government will benefit by the eventual cancellation of the principal indebtedness represented by them.

[2020 A—5]

charges for the extra-mural line
is, or would be, covered by the
£150,000 demanded by the Russians
for the repair of that line; & the P.R.
Corporation should be asked whether
they are in a position to supply any
information as to this.

Printed copies of his despatch
& enclos. to be sent to Mr. Townley.

Drafts

June 30.

H.L.B. June 22/03

In submitting the scheme to your Lordship's consideration I would beg to remark that 450,000*l.* of Chinese Indemnity Bonds would in no way represent that sum in actual fact, and that should the Railway Claim be liquidated by an allotment of the mere sum claimed in bonds, the compensation granted, in that it will not be negotiable, will fall far short of the cost of the damage done, whilst the Administration will be seriously hampered in the execution of necessary repairs and works by its inability to raise the funds immediately required for the carrying out of the same.

I have, &c.
(Signed) WALTER TOWNLEY.

Inclosure in No. 1.

Draft of Proposed Scheme for Financing the Chinese Northern Railways Indemnity.

FOR the purposes of this draft proposal it has been assumed that the Northern Railways Indemnity has been allowed at 450,000*l.*, which sum includes the claim of His Majesty's Government 90,000*l.*

1. The sum of 800,000*l.* of the British General Indemnity, originally set apart to cover the Northern Railways Indemnity Claim, shall be retained by the British Government as security for a loan to be raised to provide in cash the amount of the claim allowed—namely, 450,000*l.* The bonds representing the sum of 800,000*l.* above mentioned are hereinafter referred to as Indemnity Security Bonds.

2. Only the yearly interest—namely, 32,000*l.*—accruing on the Indemnity Security Bonds will be applied to the interest and amortization of the proposed loan. On the extinction of that loan the Indemnity Security Bonds will be free of all charge on that account, and will then be at the discretionary disposal of His Majesty's Government, and His Majesty's Government if it so desires may arrange beforehand to return them to the Chinese Government for cancellation, in which case the said Indemnity Security Bonds need not participate in the annual drawings for redemption of the British General Indemnity.

3. His Majesty's Government will not by any act of its own allow the provisions of Article 6 of the Protocol of the 7th September, 1901, so far as the same are applicable to the Indemnity Security Bonds, to be modified to their prejudice as such security.

4. The interest on the Indemnity Security Bonds as it accrues shall be handed to the Hong Kong and Shanghai Banking Corporation in London for the credit of the British and Chinese Corporation in trust, to be applied to the service of the proposed loan.

5. The proposed loan shall be evidenced by bonds of the Imperial Railways of North China for the aggregate sum of 450,000*l.*, bearing interest at 5 per cent. per annum, and shall be further secured by a second mortgage on the properties, rents, and profits of the Imperial Railways of North China, subordinated, however, to the existing first mortgage on the same.

6. It is believed that a loan thus doubly secured may be placed on the market at a price which would yield about 88 per cent. net. The difference between this price and the par value of the bonds, namely, 12 per cent., would be approximately covered by the interest which at the date of the issue of the new bonds would have already accrued on the Indemnity Security Bonds. By the 30th June, 1903, the interest accrued on the Indemnity Security Bonds will amount to 48,000*l.*, and this is not including 16,000*l.* interest due on the 31st December, 1901, payment of which is deferred in terms of the Protocol.

Stated differently, after applying the accrued interest, say 48,000*l.* to payment on account of the claim allowed, it is proposed to raise the balance of said claim—namely, 402,000*l.*—by a loan of a sum nominal sufficient to yield that amount net, or say, in round figures, a loan of 450,000*l.*

7. The interest on the Indemnity Security Bonds, namely, 32,000*l.* per annum, applied to the service of the second mortgage bonds will, on a sinking fund basis, extinguish the latter in about twenty-five years—that is to say, by means of a half-yearly payment of 352681 per cent. which, calculated on 450,000*l.* is equal to 15,866*l.*, or a total yearly payment of 31,732*l.* The small surplus of, say, 268*l.* may be applied to defray expenses of the management of the loan service.

8. The second mortgage and second mortgage bonds shall contain, among other conditions, a guarantee by the Imperial Railways of North China to make good any deficiency that may arise in the funds provided as above for the service of the proposed loan.

9. The proceeds of the proposed loan shall be paid to the Hong Kong and Shanghai Banking Corporation at London for the credit of the British and Chinese Corporation in trust, to be applied to the purposes of the Northern Railways Indemnity, and disbursements of the same shall be made only on the joint warrant of the Representative of the British and Chinese Corporation on the Railway Directorate in Tien-tsin, and of the Chief Engineer of the Imperial Railways of North China, whose appointment is provided for by the first mortgage agreement.

Peking, April 21, 1903.

(Signed)

E. G. HILLIER.

AFFAIRS OF CHINA.

[June 8.]

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CONFIDENTIAL.

SECTION 5.

No. 1.

Mr. Townley to the Marquess of Lansdowne.—(Received June 8.)

(No. 159.)
My Lord,

Peking, April 22, 1903.

I HAD the honour to inform our Lordship in my despatch No. 138 of the 9th instant, forwarding a statement of the Northern Railways Indemnity Claim, that I learned from Mr. Cousins, the representative at Tien-tsin of the British Bondholders, that the Railway was in urgent need of funds to carry out necessary repairs, and that it was hoped that His Majesty's Government would come to an early decision as to the method in which the claim should be met.

I would now beg to submit to your Lordship a scheme for the payment of the Railway Indemnity Claim, which has been drawn up by Mr. E. G. Hillier, the manager of the Peking branch of the Hong Kong and Shanghai Bank, at the request of Mr. Cousins.

Mr. Hillier points out in the preamble to his scheme that the Railway Claim was excluded from the method adopted for the payment of other private claims at the special instruction of His Majesty's Government, which reserved it for later consideration, and that if the British and Chinese Corporation was not in a position to offer a definite estimate of the damage done at the time of the adoption of the general scheme for the payment of British private claims, it was due to natural circumstances, and through no fault of their own.

Mr. Hillier further demonstrates that, as far as his information goes, whereas other private claimants were given their choice of an early payment without interest or Chinese Government 4 per cent. bonds, the British and Chinese Corporation were given no option on account of the Railway Claim, but were allotted bonds, which under any circumstances would be hard to place on the market, but which under the peculiar conditions prevailing may be considered as unmarketable. He also shows that about 30,000*l.* on account of private claims have been included in the Northern Railway Indemnity Claim by the Claims Commissioner; that if the claimants are made to accept bonds on account of their claims, they will be hardly treated as compared with other British private claimants, whereas if they are to be paid in cash the Railway Administration will be a heavy sufferer.

Mr. Hillier has based his scheme upon the assumption that His Majesty's Government may feel itself in a position to retain the whole of the 800,000*l.* set aside to cover the Railways Indemnity Claim, and he expresses a hope that should His Majesty's Government feel bound to return to the Chinese Government any portion of the above-mentioned sum, such portion should not be more than sufficient to cover the Russian claim on account of the extra-mural line, and then only in such amount as would represent the total expenditure of the Russian military authorities on capital account, after deducting net earnings during their administration of the line. A full statement of the negotiations connected with the Russian Railway Claim will form the subject of an accompanying despatch.

For the purposes of his scheme Mr. Hillier has taken the sum of 450,000*l.*, the total amount of the claim submitted to your Lordship in my despatch No. 138 of the 9th instant, on the hypothesis that such claim has been allowed, and he suggests that the 800,000*l.* originally set apart to cover the Railway Claim should be retained by His Majesty's Government as security for a loan to raise the each amount of the claim allowed, hypothetically, and that the yearly interest on the Indemnity Bonds, namely, 32,000*l.*, be applied to the interest and amortization of the proposed loan. Mr. Hillier calculates that with the sum of 48,000*l.*, which will have accrued as interest on the Railway Indemnity Bonds by the 30th June next, the total amount of the loan will have been paid off in about twenty-five years, and that His Majesty's Government will be free to do what it judges suitable with the 800,000*l.* of bonds earmarked for the Railway Claim. Mr. Hillier calls attention, in the concluding paragraph of his preamble, to the fact that by his scheme the 800,000*l.* of Indemnity Bonds on the Railways account will be excluded from the drawings for redemption of the British Indemnity, while the Chinese Government will benefit by the eventual cancellation of the principal indebtedness represented by them.

[2020 A—5]

Mr. Townley
No. 159
April 22. 1903
Rec'd June 8
[in No 138 of April 9. 1903]
Northern Railways
Indemnity claims
Mr. Mr. Hillier's scheme
for payment of :-

In submitting the scheme to your Lordship's consideration I would beg to remark that 450,000*l.* of Chinese Indemnity Bonds would in no way represent that sum in actual fact, and that should the Railway Claim be liquidated by an allotment of the mere sum claimed in bonds, the compensation granted, in that it will not be negotiable, will fall far short of the cost of the damage done, whilst the Administration will be seriously hampered in the execution of necessary repairs and works by its inability to raise the funds immediately required for the carrying out of the same.

I have, &c.
(Signed) **WALTER TOWNLEY.**

Inclosure in No. 1.

Draft of Proposed Scheme for Financing the Chinese Northern Railways Indemnity.

FOR the purposes of this draft proposal it has been assumed that the Northern Railways Indemnity has been allowed at 450,000*l.*, which sum includes the claim of His Majesty's Government 90,000*l.*

1. The sum of 800,000*l.* of the British General Indemnity, originally set apart to cover the Northern Railways Indemnity Claim, shall be retained by the British Government as security for a loan to be raised to provide in cash the amount of the claim allowed—namely, 450,000*l.* The bonds representing the sum of 800,000*l.* above mentioned are hereinafter referred to as Indemnity Security Bonds.

2. Only the yearly interest—namely, 32,000*l.*—accruing on the Indemnity Security Bonds will be applied to the interest and amortization of the proposed loan. On the extinction of that loan the Indemnity Security Bonds will be free of all charge on that account, and will then be at the discretionary disposal of His Majesty's Government, and His Majesty's Government if it so desires may arrange beforehand to return them to the Chinese Government for cancellation, in which case the said Indemnity Security Bonds need not participate in the annual drawings for redemption of the British General Indemnity.

3. His Majesty's Government will not by any act of its own allow the provisions of Article 6 of the Protocol of the 7th September, 1901, so far as the same are applicable to the Indemnity Security Bonds, to be modified to their prejudice as such security.

4. The interest on the Indemnity Security Bonds as it accrues shall be handed to the Hong Kong and Shanghai Banking Corporation in London for the credit of the British and Chinese Corporation in trust, to be applied to the service of the proposed loan.

5. The proposed loan shall be evidenced by bonds of the Imperial Railways of North China for the aggregate sum of 450,000*l.*, bearing interest at 5 per cent. per annum, and shall be further secured by a second mortgage on the properties, rents, and profits of the Imperial Railways of North China, subordinated, however, to the existing first mortgage on the same.

6. It is believed that a loan thus doubly secured may be placed on the market at a price which would yield about 88 per cent. net. The difference between this price and the par value of the bonds, namely, 12 per cent., would be approximately covered by the interest which at the date of the issue of the new bonds would have already accrued on the Indemnity Security Bonds. By the 30th June, 1903, the interest accrued on the Indemnity Security Bonds will amount to 48,000*l.*, and this is not including 16,000*l.* interest due on the 31st December, 1901, payment of which is deferred in terms of the Protocol.

Stated differently, after applying the accrued interest, say 48,000*l.* to payment on account of the claim allowed, it is proposed to raise the balance of said claim—namely, 402,000*l.*—by a loan of a sum nominal sufficient to yield that amount net, or say, in round figures, a loan of 450,000*l.*

7. The interest on the Indemnity Security Bonds, namely, 32,000*l.* per annum, applied to the service of the second mortgage bonds will, on a sinking fund basis, extinguish the latter in about twenty-five years—that is to say, by means of a half-yearly payment of 352581 per cent. which, calculated on 450,000*l.* is equal to 15,868*l.*, or a total yearly payment of 31,732*l.* The small surplus of, say, 268*l.* may be applied to defray expenses of the management of the loan service.

8. The second mortgage and second mortgage bonds shall contain, among other conditions, a guarantee by the Imperial Railways of North China to make good any deficiency that may arise in the funds provided as above for the service of the proposed loan.

9. The proceeds of the proposed loan shall be paid to the Hong Kong and Shanghai Banking Corporation at London for the credit of the British and Chinese Corporation in trust, to be applied to the purposes of the Northern Railways Indemnity, and disbursements of the same shall be made only on the joint warrant of the Representative of the British and Chinese Corporation on the Railway Directorate in Tientsin, and of the Chief Engineer of the Imperial Railways of North China, whose appointment is provided for by the first mortgage agreement.

Peking, April 21, 1903.

(Signed) **E. G. HILLIER.**

● requested that in any further communication on this subject the under-mentioned letter and number may be quoted, and the reply addressed to—
The Under Secretary of State for India,
India Office,
Whitehall,
London, S.W.
M. 4405.

India Office, 176
Whitehall,
London, S.W.
22nd April 1903.

Sir,

In continuation of my letter of 30th January 1903 No. M. 1425, I am directed to forward a copy of a letter from the General Officer Commanding, Tientsin, enclosing maps and plans of a section of the Imperial Railways of North China.

Dated 17th
Feb. '03
and encs.

I have the honour to be, Sir,

Your obedient Servant,

Medhurst
Major-General,

Military Secretary.

The Under Secretary of State,
Foreign Office.

*Mr. Heller's scheme
for purchase of
Imperial Railways
China.*

HEADQUARTERS STAFF OFFICE,
INTELLIGENCE BRANCH,
CHINA FORCE.

Confidential.

No. 73/I.C. Tientsin, 17th February 1903.

From the General Officer Commanding,
China Force.
To the Right Honourable
The Secretary of State for India,
India Office,
London, S.W.

My Lord,

In continuation of my No. 795/I.C. dated 20th
November 1902, I have the honour to submit for Your

Lordship's information a copy of
the marginally noted report with
maps and plans.

2. The only available copies of the photographs accompanying the Report have been forwarded to the Director-General of Military Intelligence, War Office.

I have &c.,

(Sd.) O'M. Creagh, Major-General,
Commanding China Force.

Military Report on the
Imperial Railways of
North China, Shan-Hai-
Kuan to Ying-K'ou (Niu-
Chuang), Section compiled
in the Intelligence
Branch, China Force -
February, 1903.

Q. April 23, 1903.
[reg. I. O. Jan 30, 03].

Imperial Railways in North

China.

two copies of a letter

from G. O. C. Tientem ending
maps a plan of a section of.

The greater part of
this Report is of purely
political interest, but
see pages 26, 28, 32
39. and they attended
to the Report. of the sort
and in the place and other
things.
I refered to the
fact of General's in the
463 Co. 80m. 210

~~India Office~~
G. R. } April 23, 1903

[ref I.O. Jan 30, 03]

Imperial Railways of North China

Enc. copy of letter from
G.O.C. Tientsin enclosing maps
& plans of a section of

Encl. in I.O.

April 23, 03

LIST OF MAPS AND PLANS.

173

MAPS.

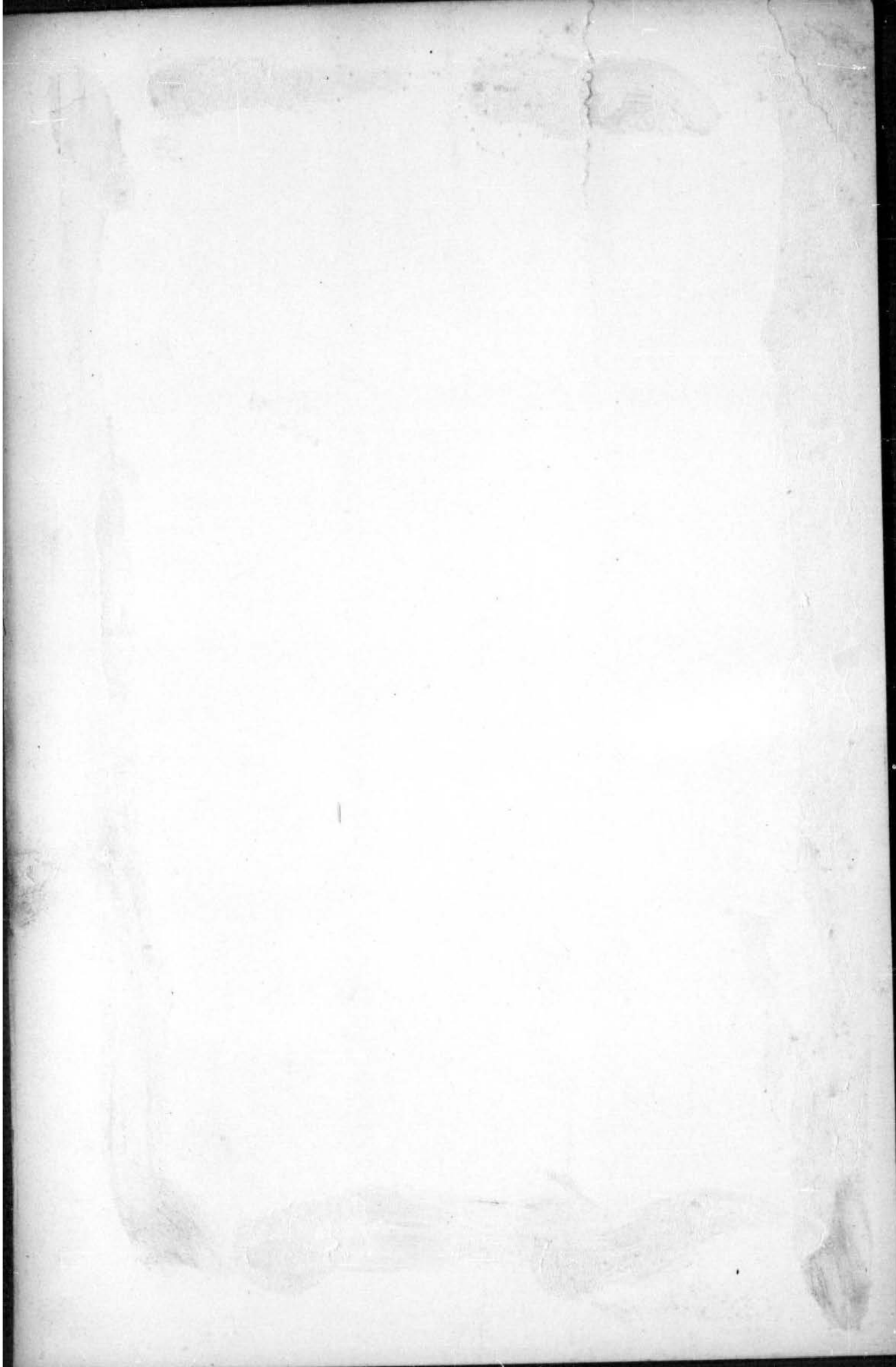
- I. SKETCH MAP to illustrate Present and Future Railways in NORTH CHINA - scale - 78 miles = 1 inch.
- II. General Map of SHENG-KING PROVINCE and Neighbouring Country - scale - 10 miles = 1 inch.
- III. Map of the Line from SHAN-HAI-KUAN to YING-K'OU (NIU-CHUANG) and HSIN-MIN-T'UN. scale - 2 miles = 1 inch.

PLANS.

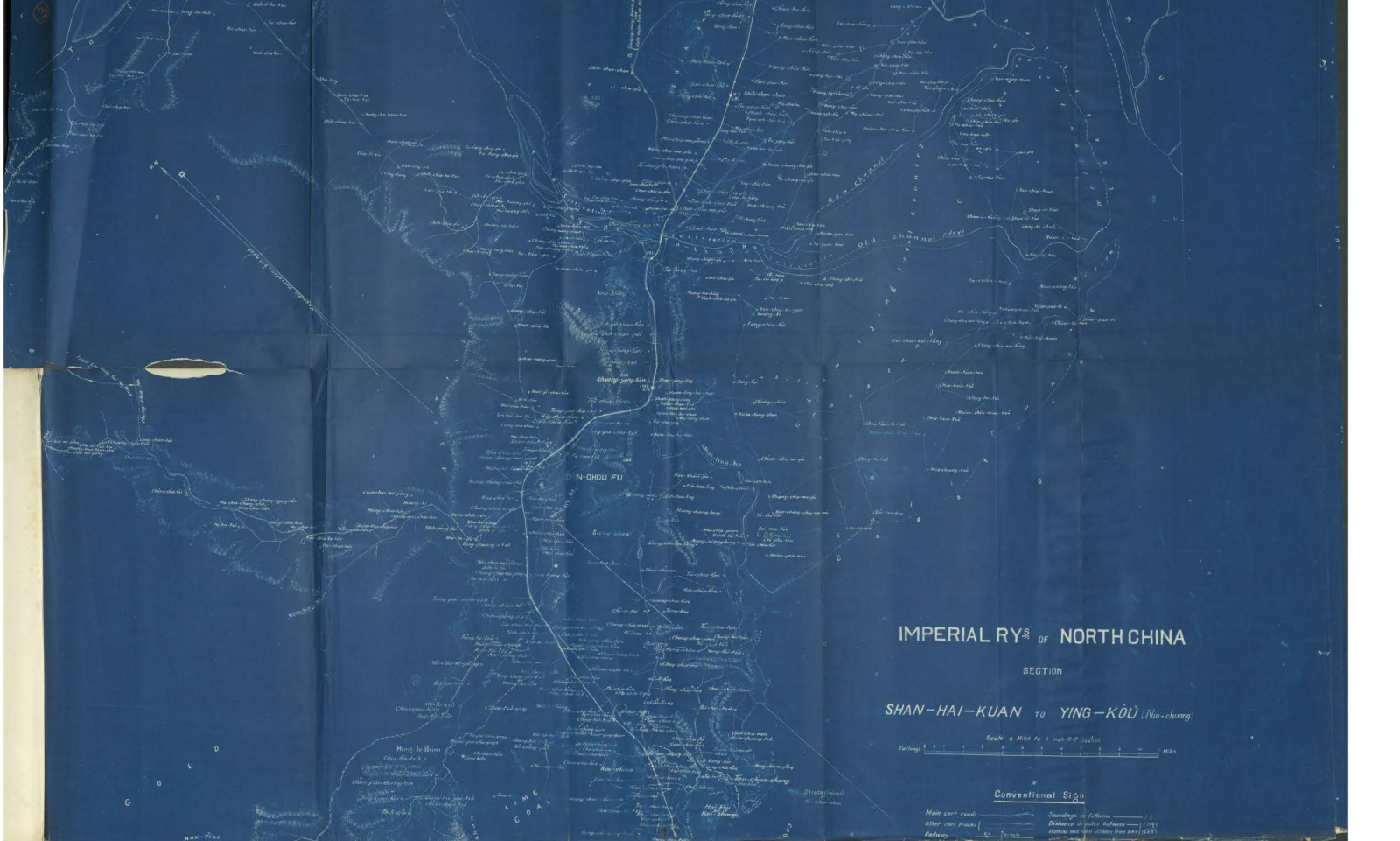
1. SHAN-HAI-KUAN STATION.
2. CH'IEN-SO STATION. (Note - "so" means a place).
3. CH'IEN-WEI STATION.
4. CH'UNG-HOU-SO STATION.
5. SHA-HO-SO STATION.
6. NING-YUAN CHOU STATION.
7. LIEN-SHAN STATION.
8. KAO-CH'IAO STATION.
9. CHIN-CHOU FU STATION.
10. TA-LING HO STATION.
11. SHIH-SHAN-CHAN STATION.
12. KOU-PANG-TZU STATION.
13. SHUANG-T'AI-TZU STATION.
14. TIEN-CHUANG-T'AI STATION.
15. YING-K'OU (NIU-CHUANG) STATION.
16. Plan of CHIN-CHOU FU.
17. Rough plan of country North of CHIN-CHOU Railway Station to illustrate points from which photographs were taken.
18. Plan of TA-LING HO Bridge.
19. Types of Cirders manufactured in SHAN-HAI-KUAN Bridge works.

Note - Map No: II has been compiled from Map No: III, from original work of members of the China Force and from Japanese and Russian Maps. The Longitude and Latitude of SHAN-HAI-KUAN and NIU-CHUANG is from Admiralty Charts.

Map No: III is, with the exception of the railway from SHAN-HAI-KUAN to CH'UNG-HOU-SO, original work. The country between NING-YUAN CHOU and the NU-ERH/HO was triangulated so as to join up the coast surveys with those of the Railway roads.







IMPERIAL RY OF NORTH CHINA

SECTION

SHAN-HAI-KUAN TO YING-K'OU (Niu-chuang)

Scale 1 Mile to 1 inch R.F. 1:125,000
Furlongs 1 2 3 4 5 6 7 8 9 10 Miles

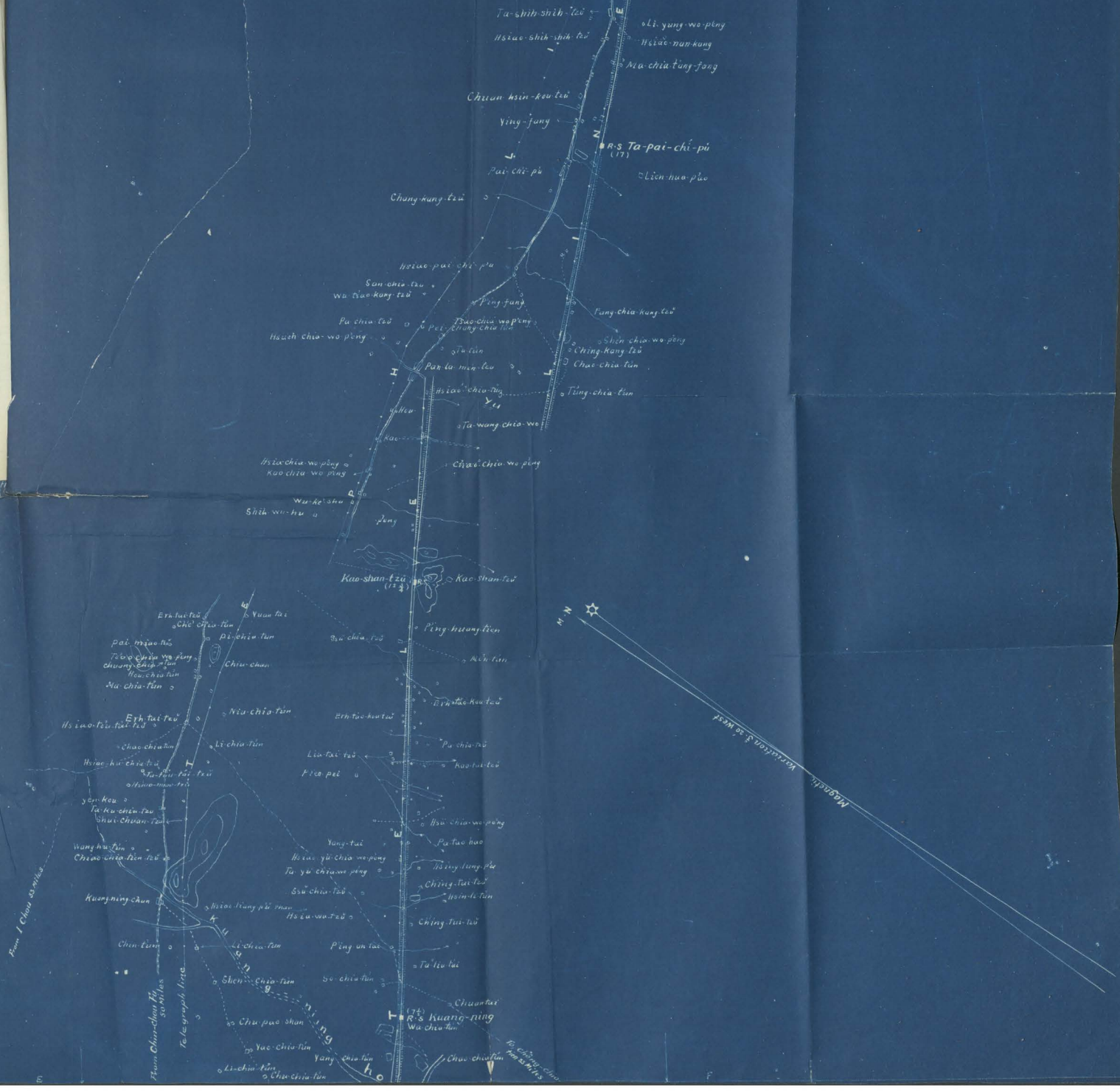
Conventional Sign

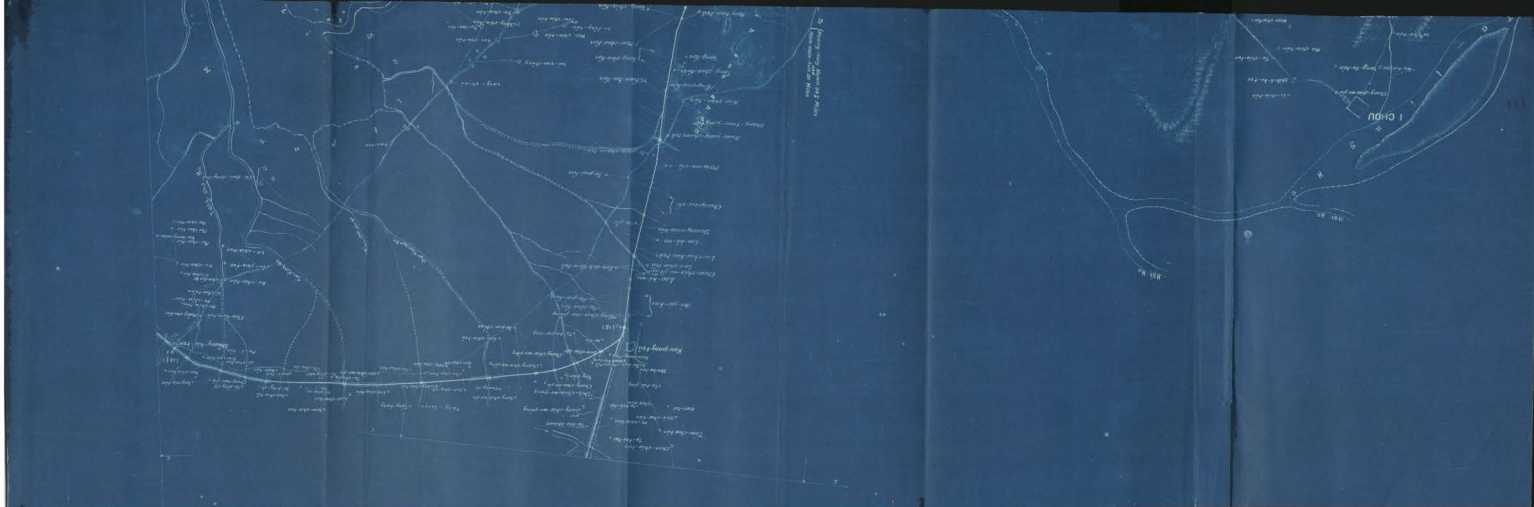
Main cart roads
Other cart roads
Railway
Soundings in fathoms
Distances in miles between points
Distances in kilometers between points

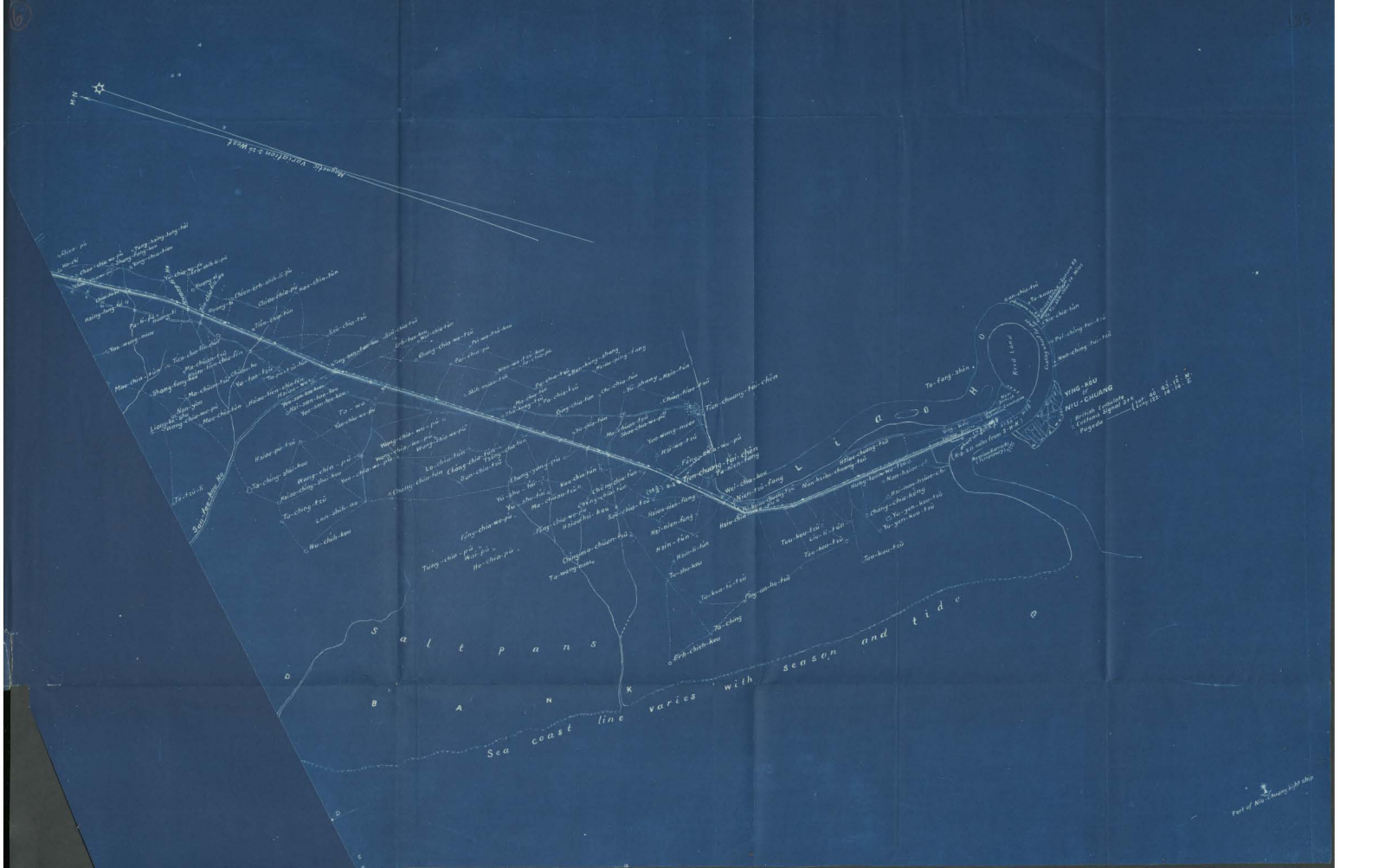
IMPERIAL RY^S OF NORTH CHINA

KOUPANG-TZŪ TO HSIN-MIN-TŪN

Scale 2 Miles to 1 inch R.F. 1:26720





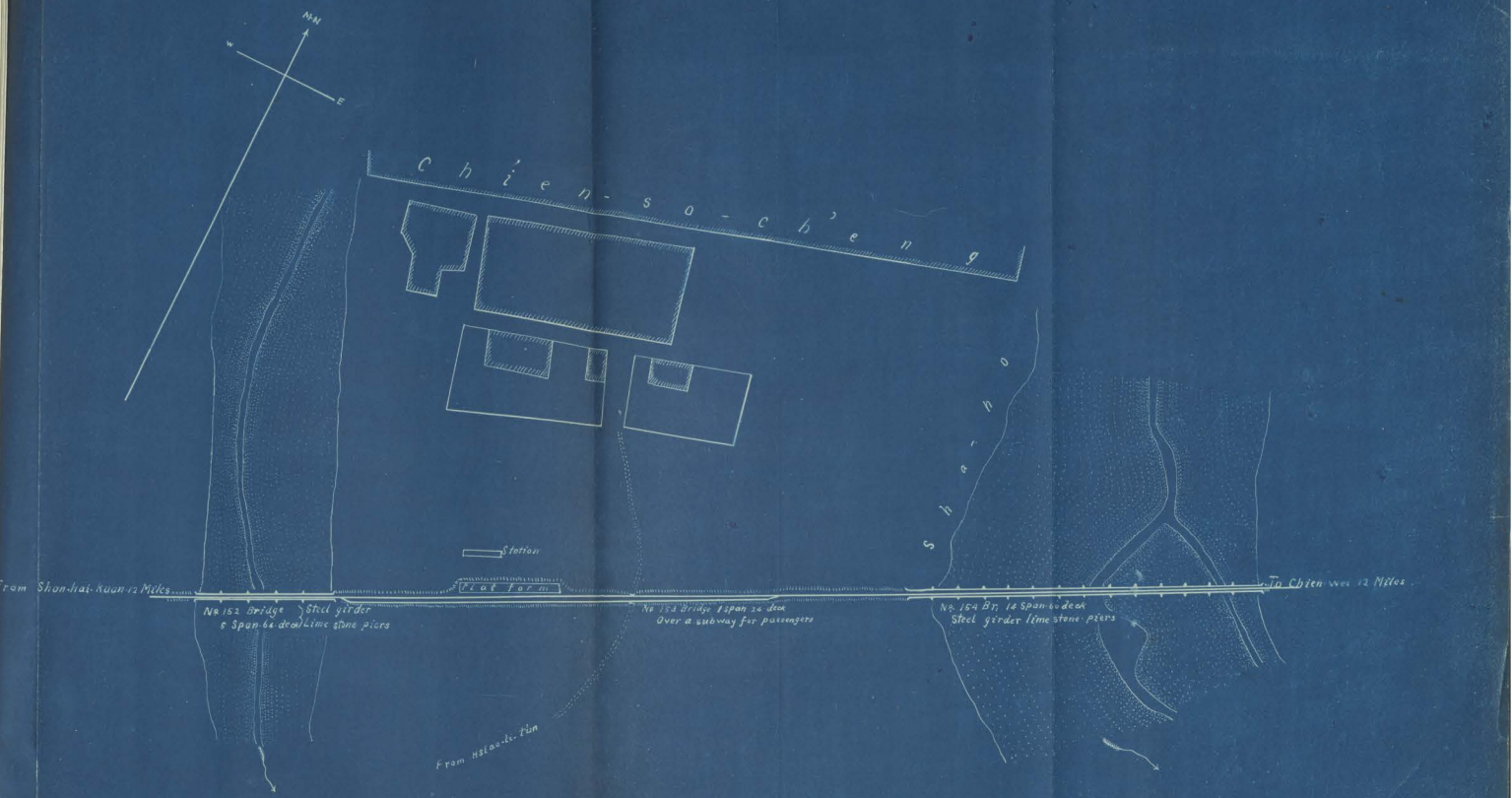


Part of Nieuwenhuis' sketch

PLAN OF CHIEN SO CHENG RAILWAY STATION

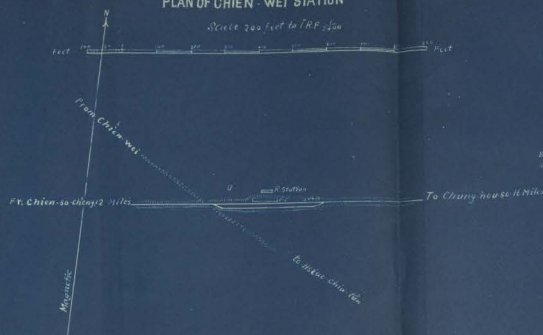
Scale 200 feet to 1 inch R.F. 1/400

Feet 0 1 2 3 4 5 6 7 8 9 10 Feet



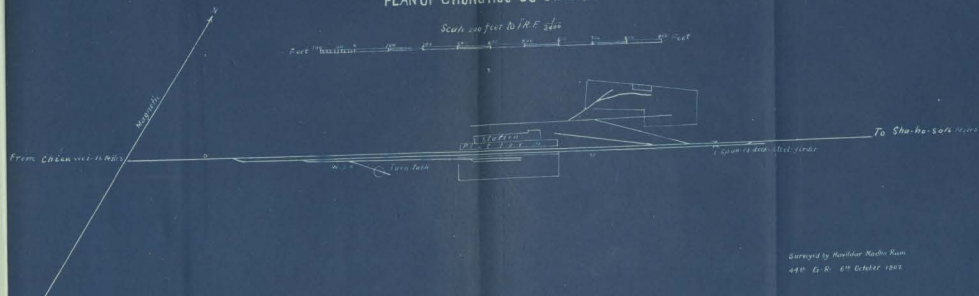
PLAN OF CHIEN-WEI STATION

Scale 200 feet to 1 inch



PLAN OF CHUNG-HOU-SO STATION

Scale 200 feet to 1 inch



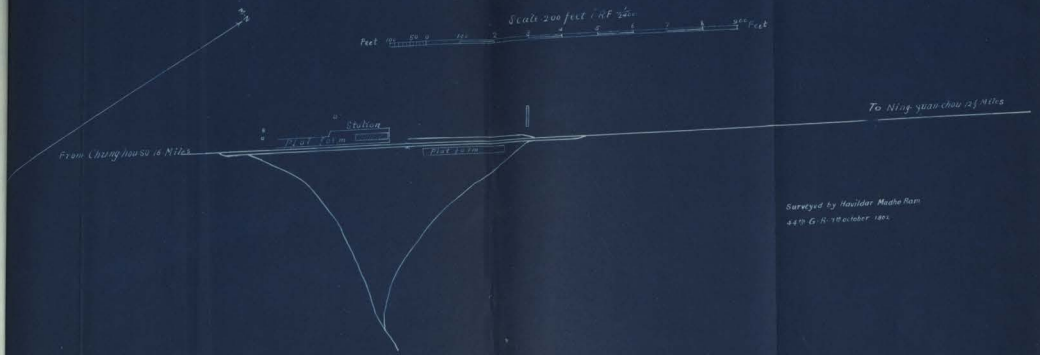
3

4

PLAN OF SHA-HO-SO STATION

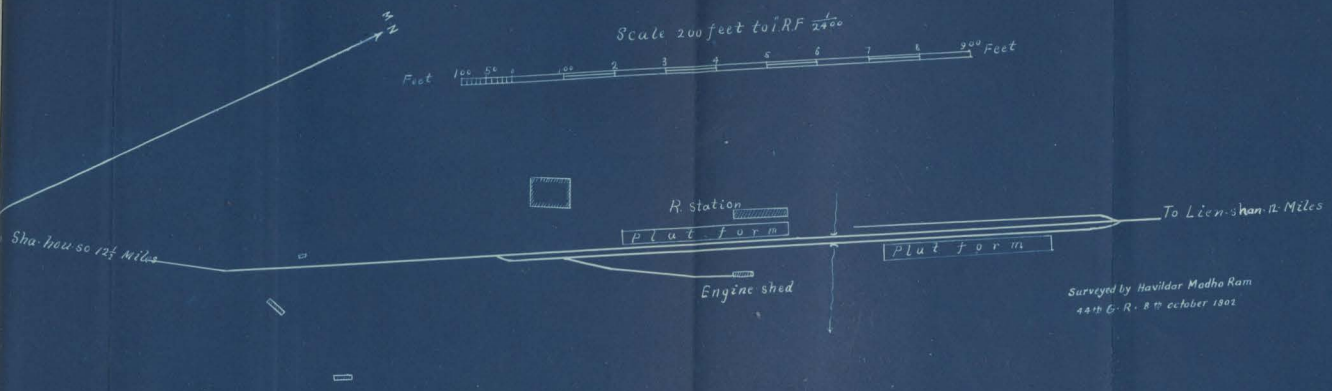
Scale 200 feet = 1 inch

Feet 0 100 200 300 400 500 600 700 800 900 1000



Surveyed by Maulder Matheram
417 G. N. 17 October 1901

PLAN OF NING-YÜAN CHOU STATION

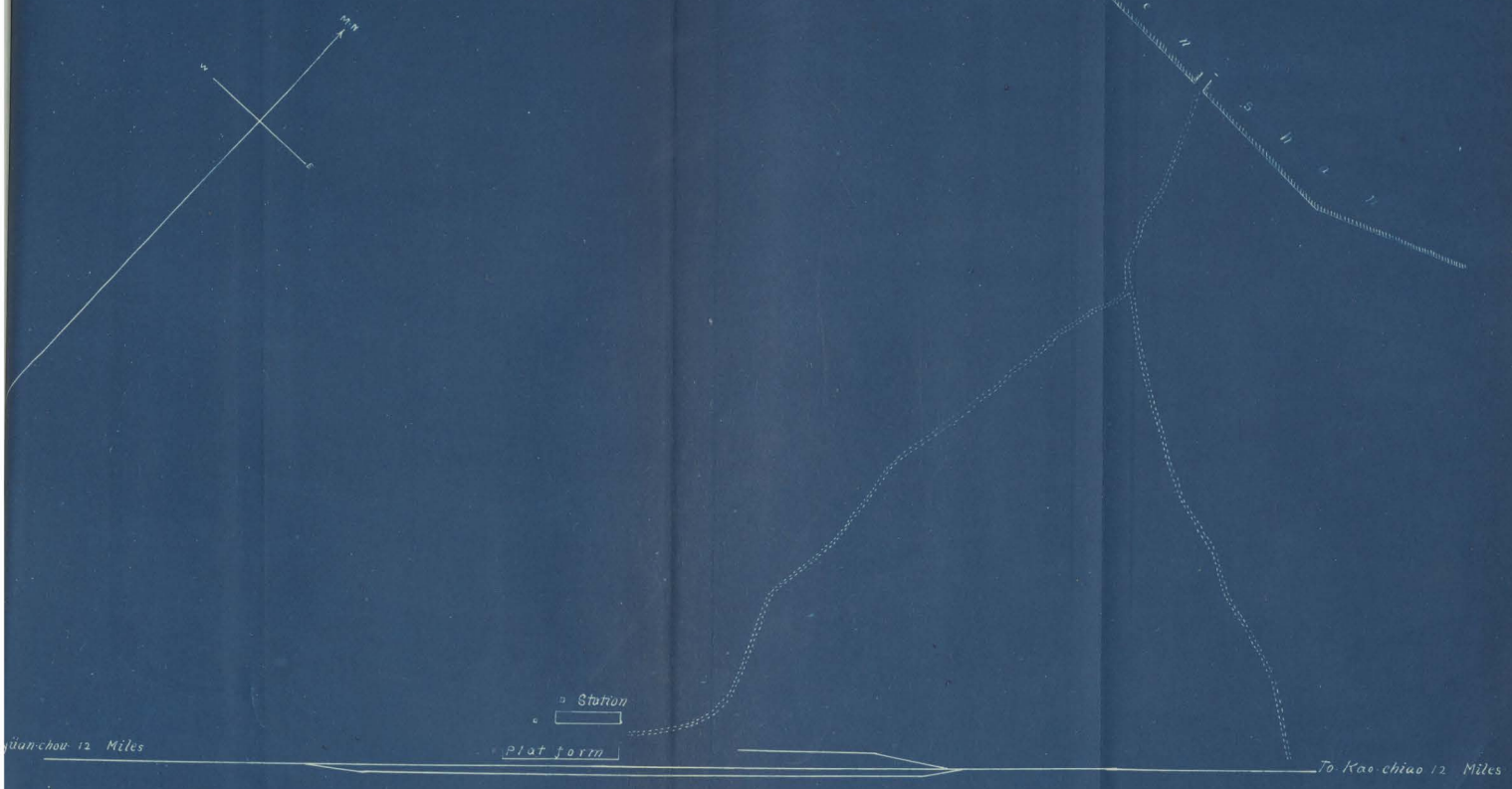


Surveyed by Havildar Madho Ram
44th E. R. 8th October 1902

PLAN OF LIEN SHAN RAILWAY STATION

Scale 200 feet to 1 inch R.F. 1/450

Feet 0 100 200 300 400 500 600 700 800 900 1000 Feet



Surveyed by Mr. Havildar Lachman Narayan
1st Brahmins B.L. 10th October 1902

PLAN OF KAO CHIAO RAILWAY STATION

Scale 200 feet to 1 inch R.F. 1/400

Feet 100 200 300 400 500 600 700 800 900 1000

K a o - c h i a o

Railway quarter

Station

Platform

Shun 12 Miles

To Chin chow fu 18 Miles

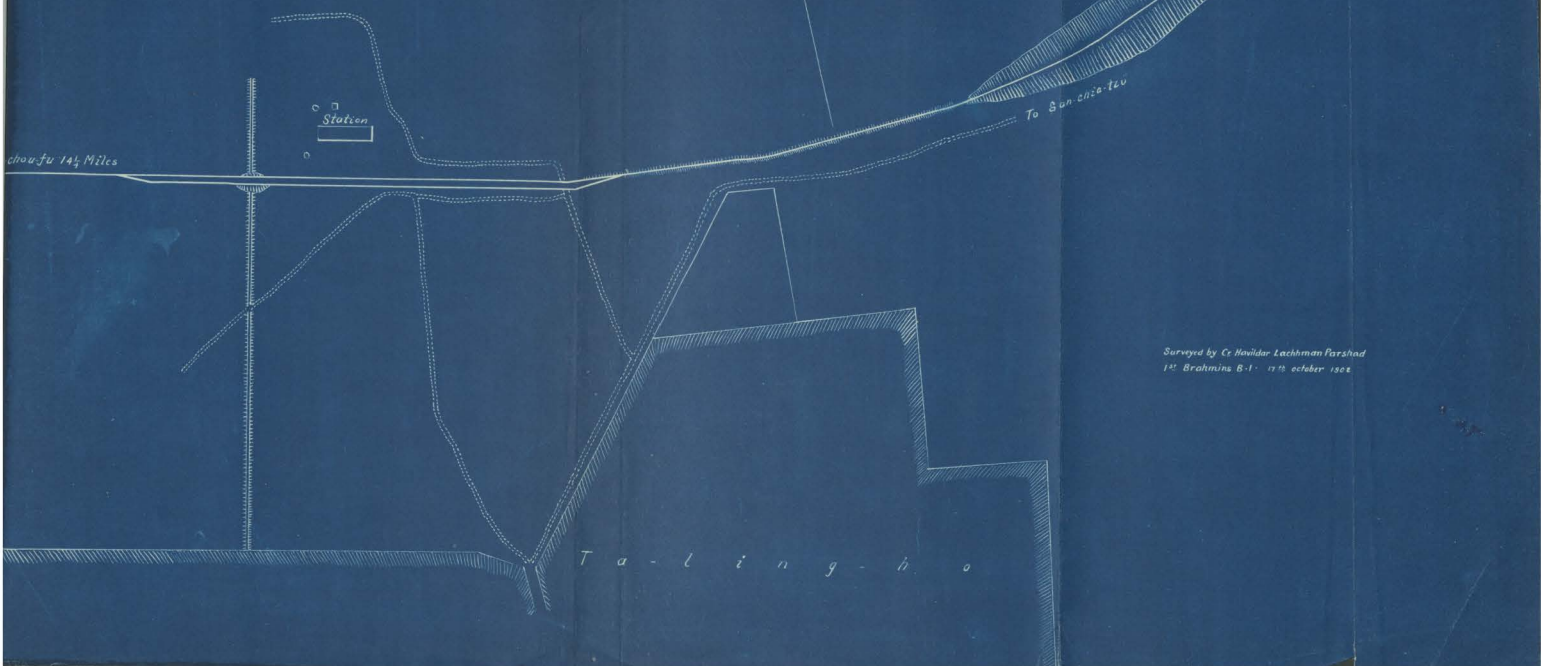
Surveyed by Mr. Havildar Lachman Parshad
1st Brigadier B.I. 11th October 1902

To Tao chih

PLAN OF TA LING HO STATION

Scale 200 feet to 1 inch

Feet 100 200 300 400 500 600 700 800 900 1000 Feet



Surveyed by Mr. Haulder Lachman Parshad
1st Brahmins B.I. 17th October 1906

PLAN OF SHIH SHAN CHAN STATION

Scale 200 ft to 1 inch
Feet 100 200 300 400 500 600 700 800 900 1000

From Ta ling ho 9 1/2 miles

Single

Station

Single

Water pump

To Kao peng tai 15 miles

Surveyed by Nash Man Singh
37 m Degros 21st October 1902

From Ning shan

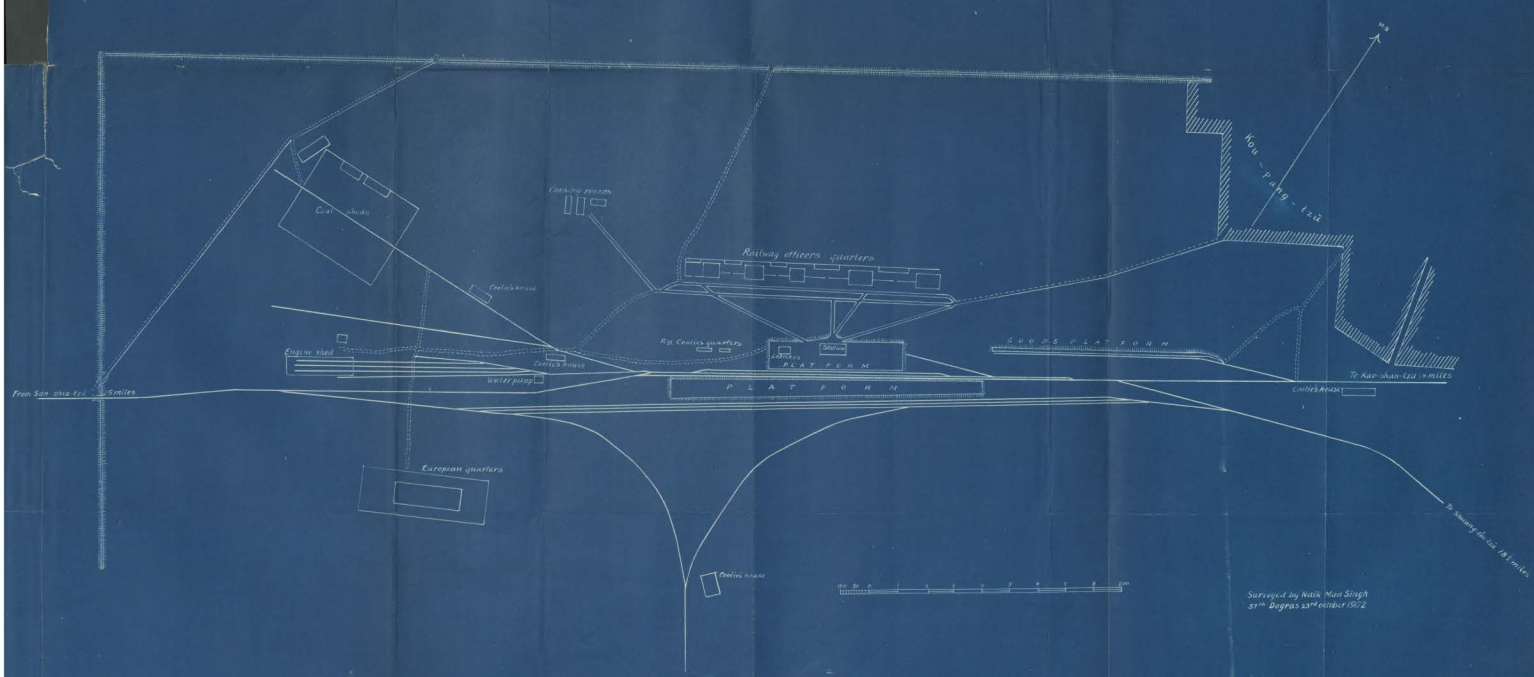
To San chi tai
To San chi tai

Magnetic

PLAN OF KOU-PANG-TZŪ STATION

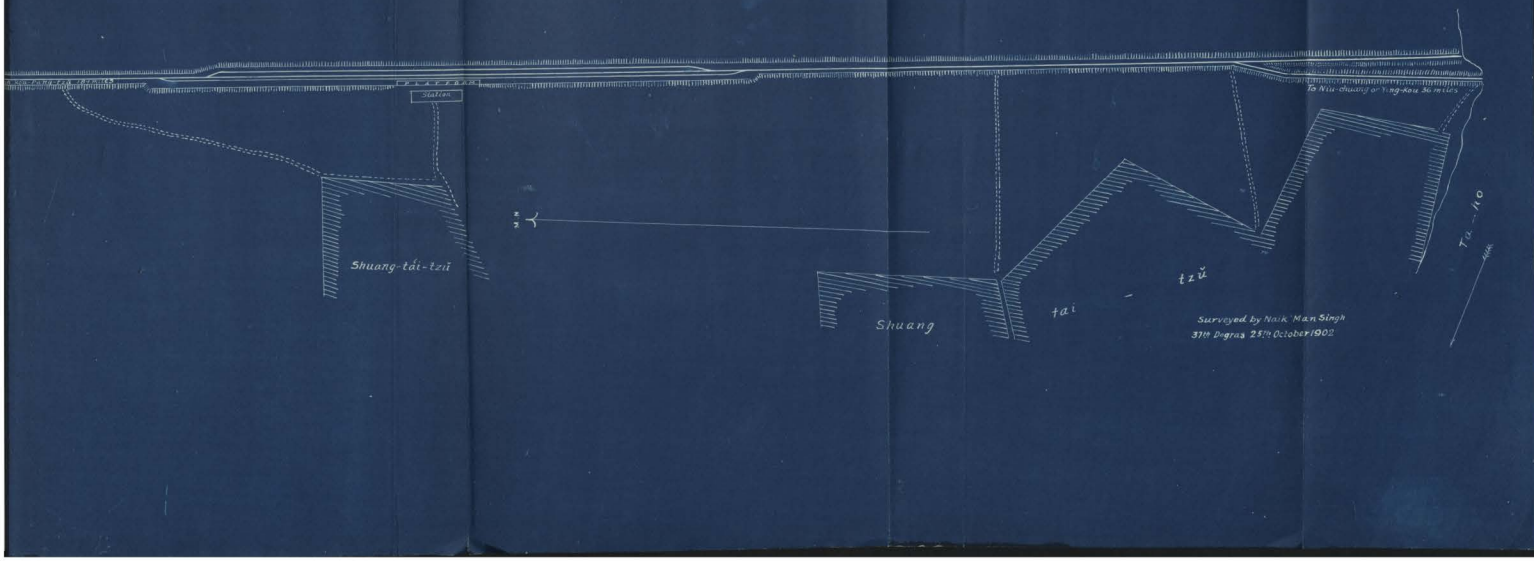
Scale 2000 feet to 1 inch

0 100 200 300 400 500 600 700 800 900 1000 feet



PLAN OF SHUANG TAI TZŪ STATION

Scale 100 ft. to 1 inch
Foot 100 ft. to 1 inch

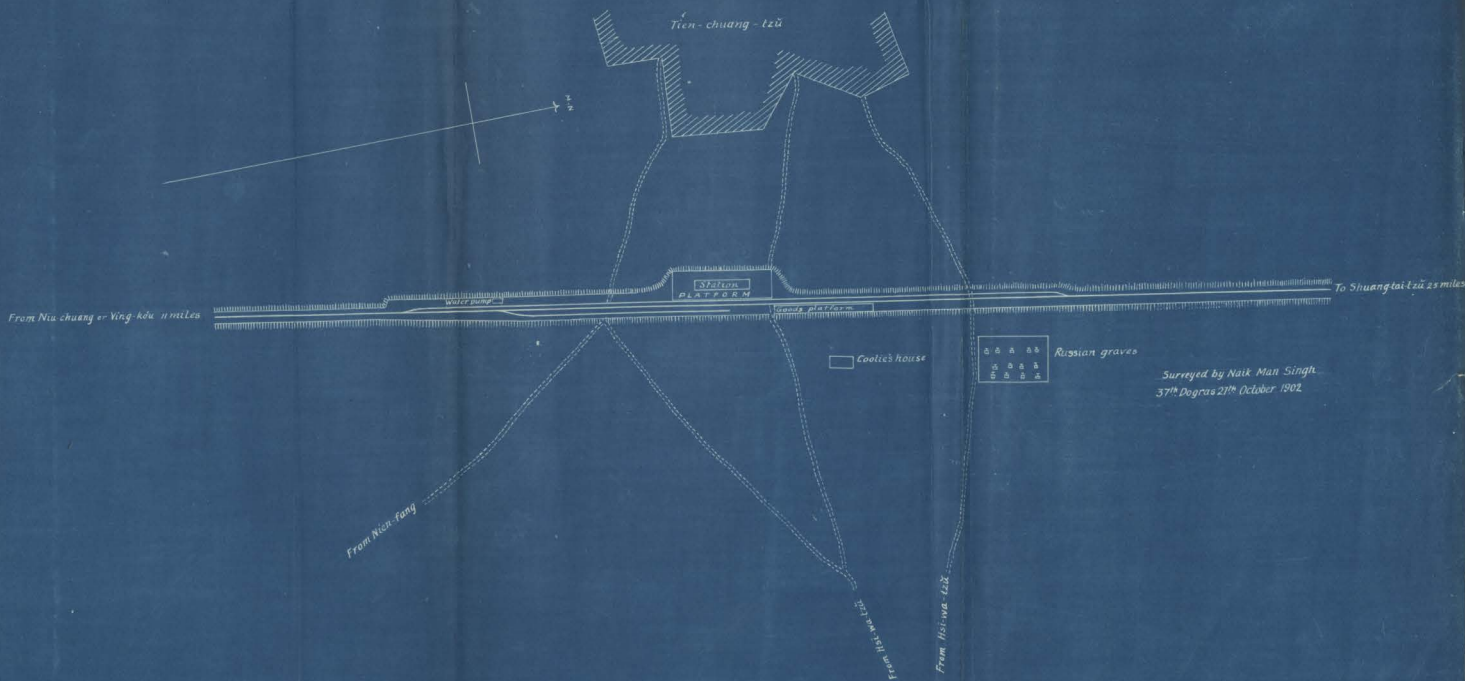


Surveyed by Nick Man Singh
31st August 25th October 1902

PLAN OF TIEN CHUANG TAI STATION

197

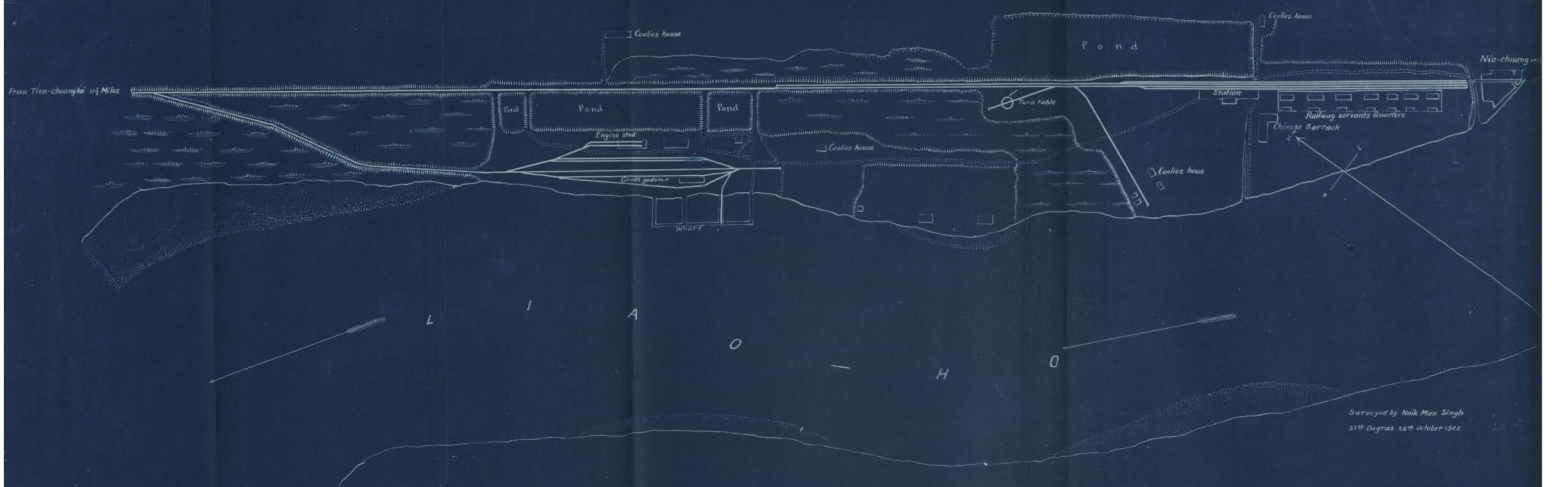
Scale 1:50,000
Feet 100 200 300 400 500 600 700 Feet



PLAN OF NIU CHUANG (YING KOU) RAILWAY STATION

Scale 400 feet to 1 inch 5-5" dia

Feet 0 100 200 300 400 500 600 700 800 900 1000

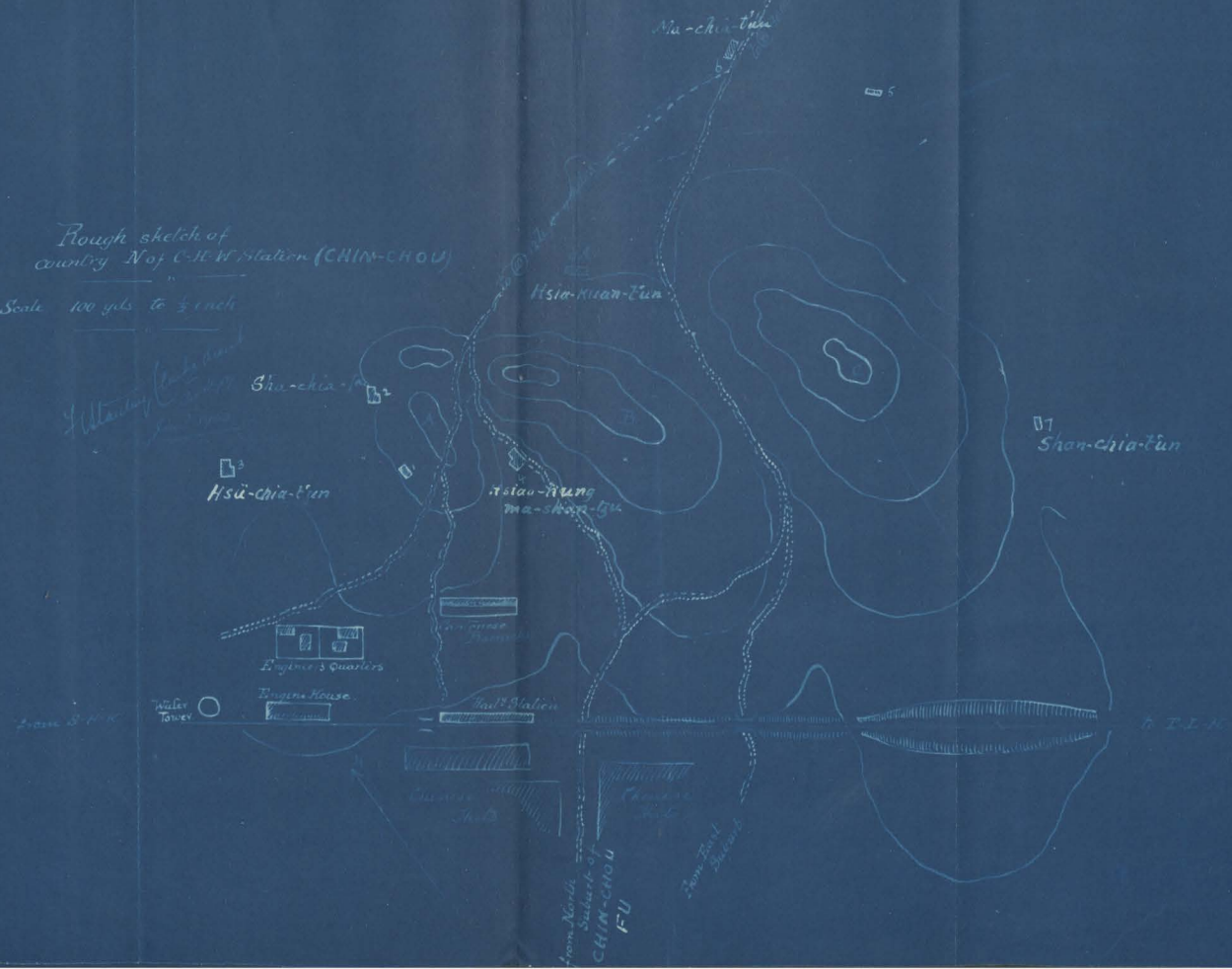




Rough sketch of
country N of C.H.W. Station (CHIN-CHOU)

Scale 100 yds to 1/2 inch

Following (by road)
from N. to S.



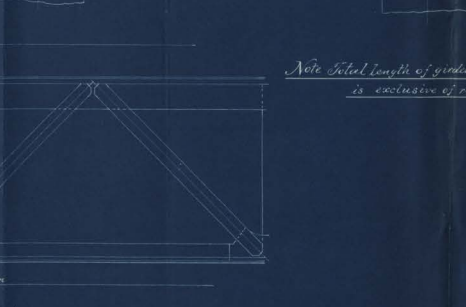
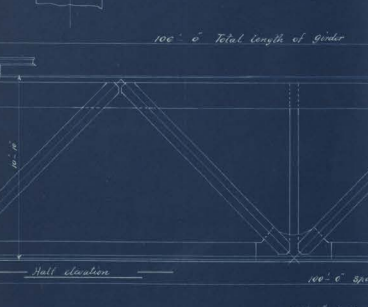
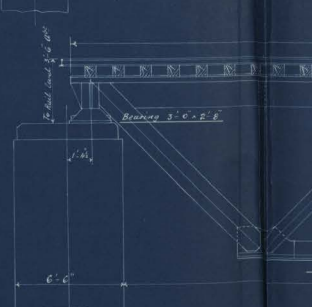
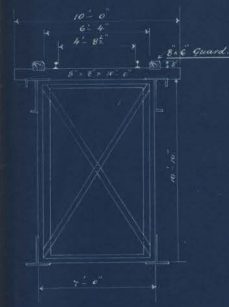
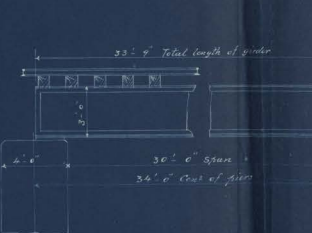
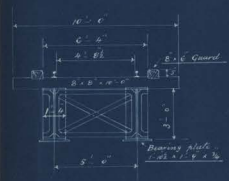
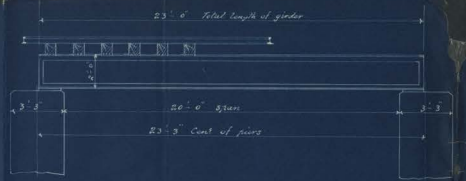
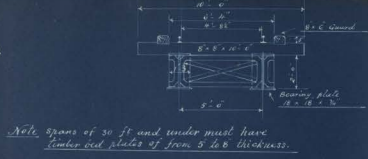
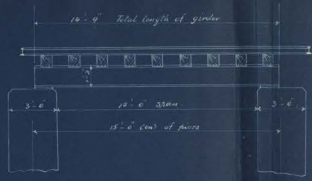
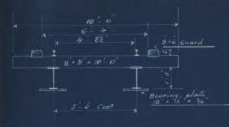


Diagram D1

Scale: 1/4" = 1 ft

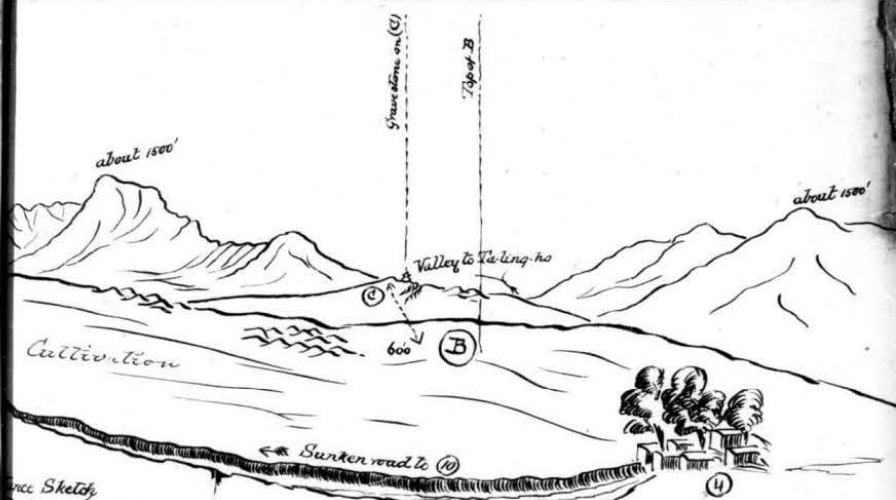
I. C. R.
STANDARD STEEL GIRDERS.

Note: Spans of 30 ft and under must have timbered end plates of from 5 to 8 thickness.

Note: Total length of girders given is exclusive of rivet heads.

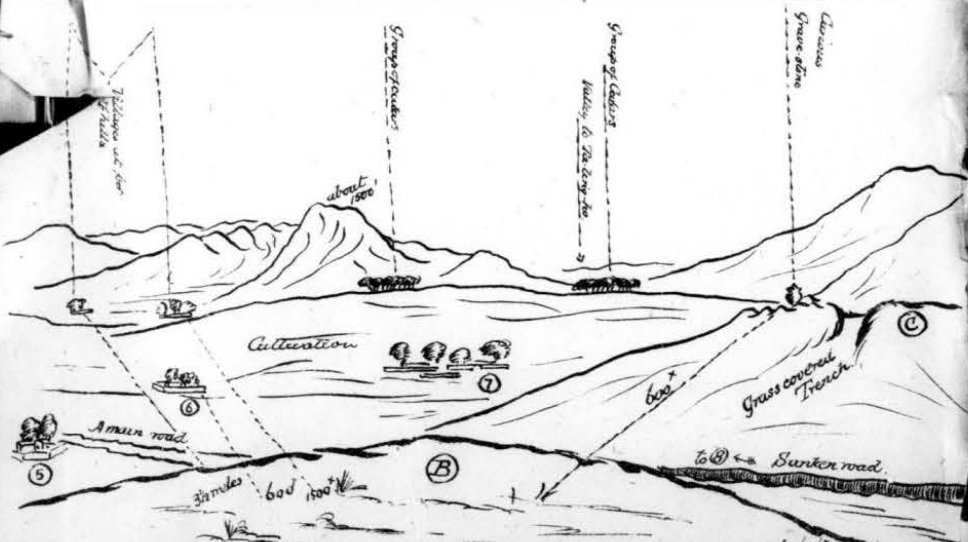
This sketch is taken from the East side
of the remarkable grave stone
shewn in the other two sketches.

W. Stanley Jones
Dec. 30 1901.
Jan 2 1903



Sketch
of Chen-chou-fou R^o 30"
from A looking N.E. - E + S.E. across B + C

Director Cash Account
30th P.I. Jan? 1903.



Distance Sketch
See Map of Chen-chow-fu RY 36
View from B looking E & SE across C.

William H. Keady, Jr.
30 N. 7th. June 1903

Trees & Villages in the distance

Height 100'

Banks about 4 feet high

Height 100'

Mean Height 100 feet

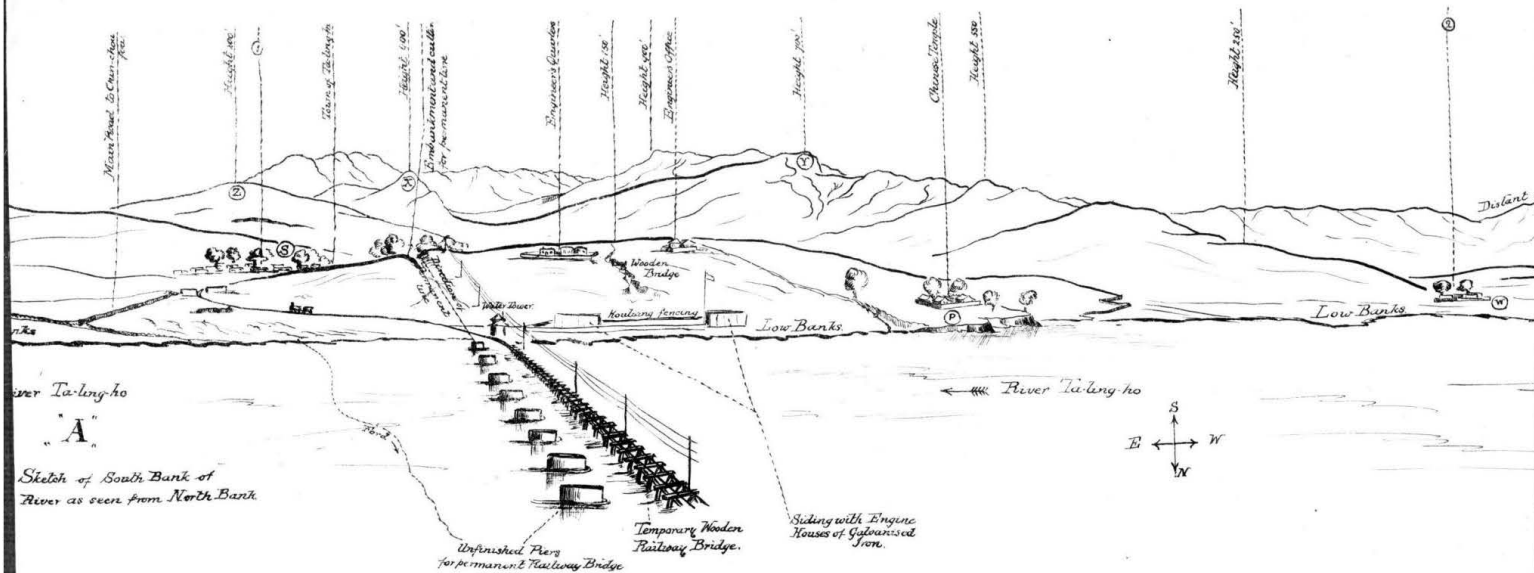
Low Banks

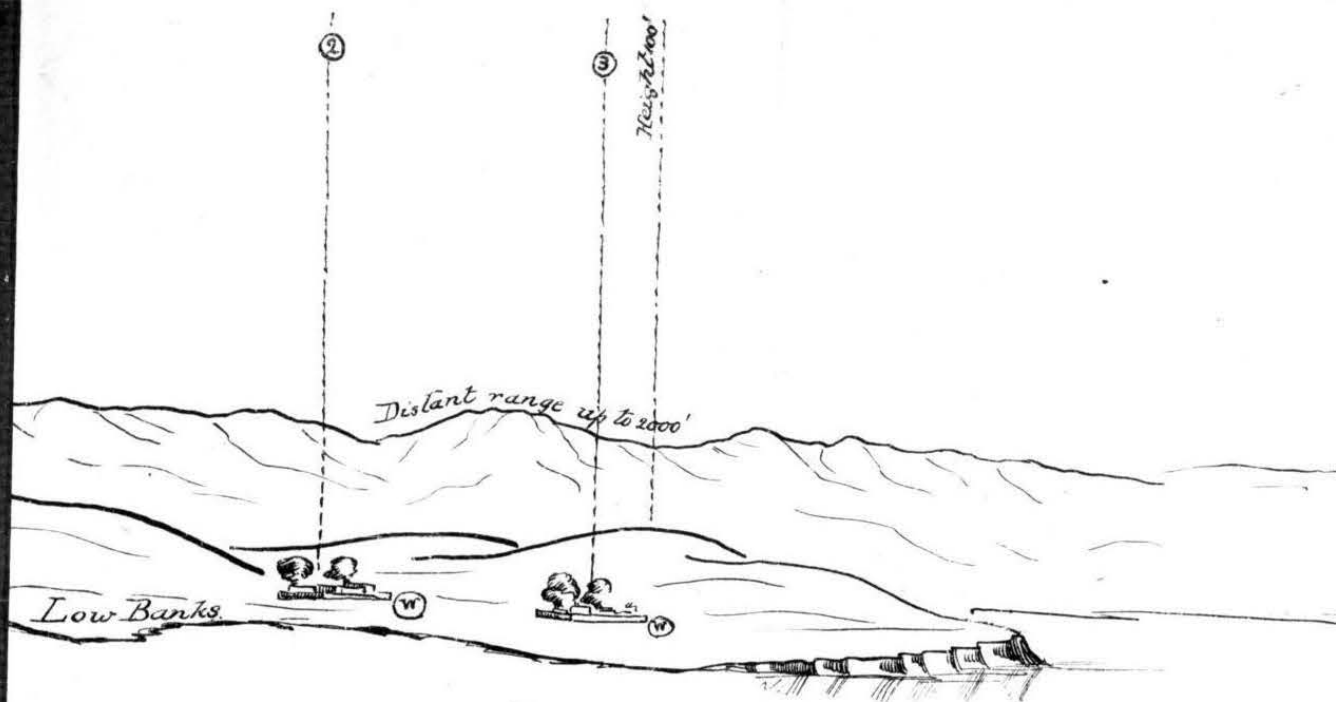
River Ta-ling-ho

River Ta-ling-ho

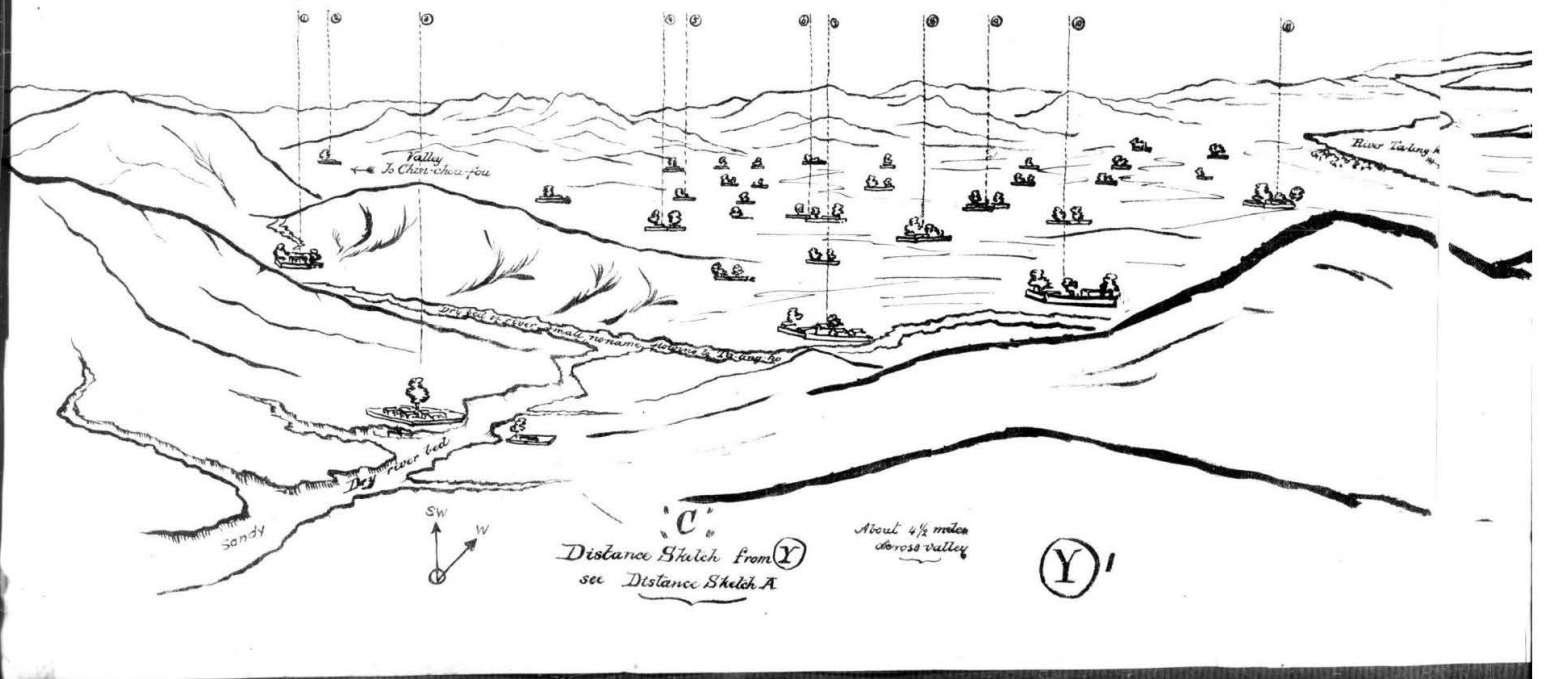
"A"

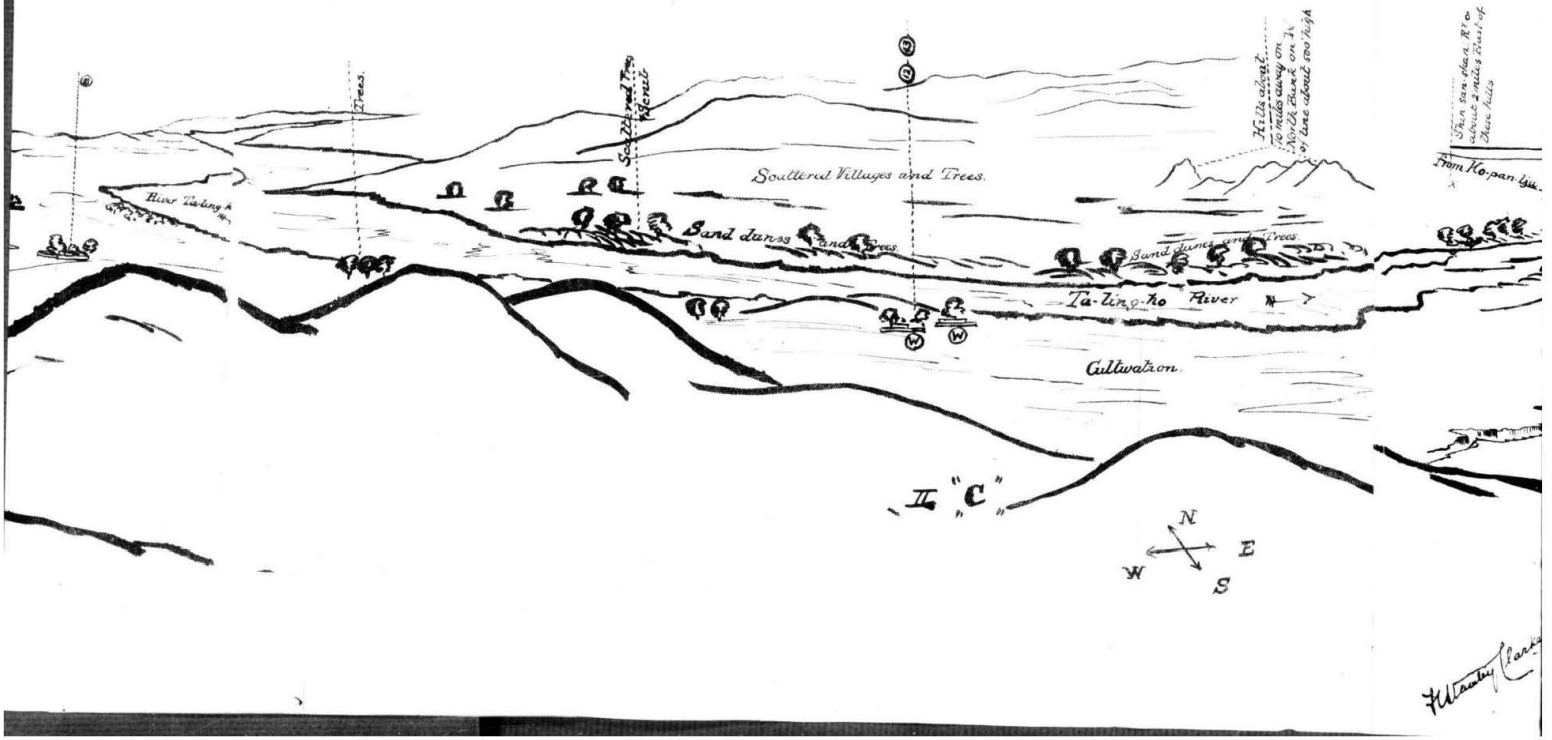
Distance Sketch of South
Ta-ling-ho River as seen from

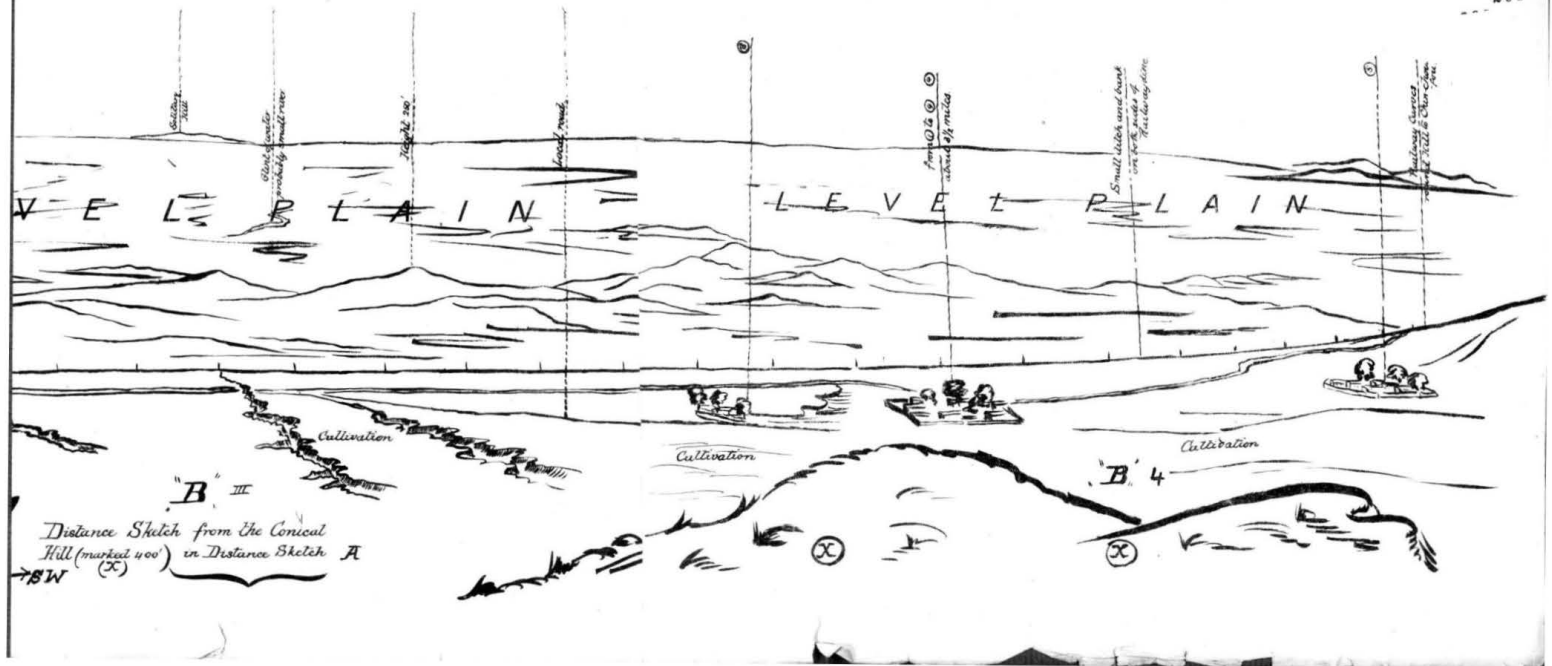




William Clark
30th P.I.
Jan 2 1903.







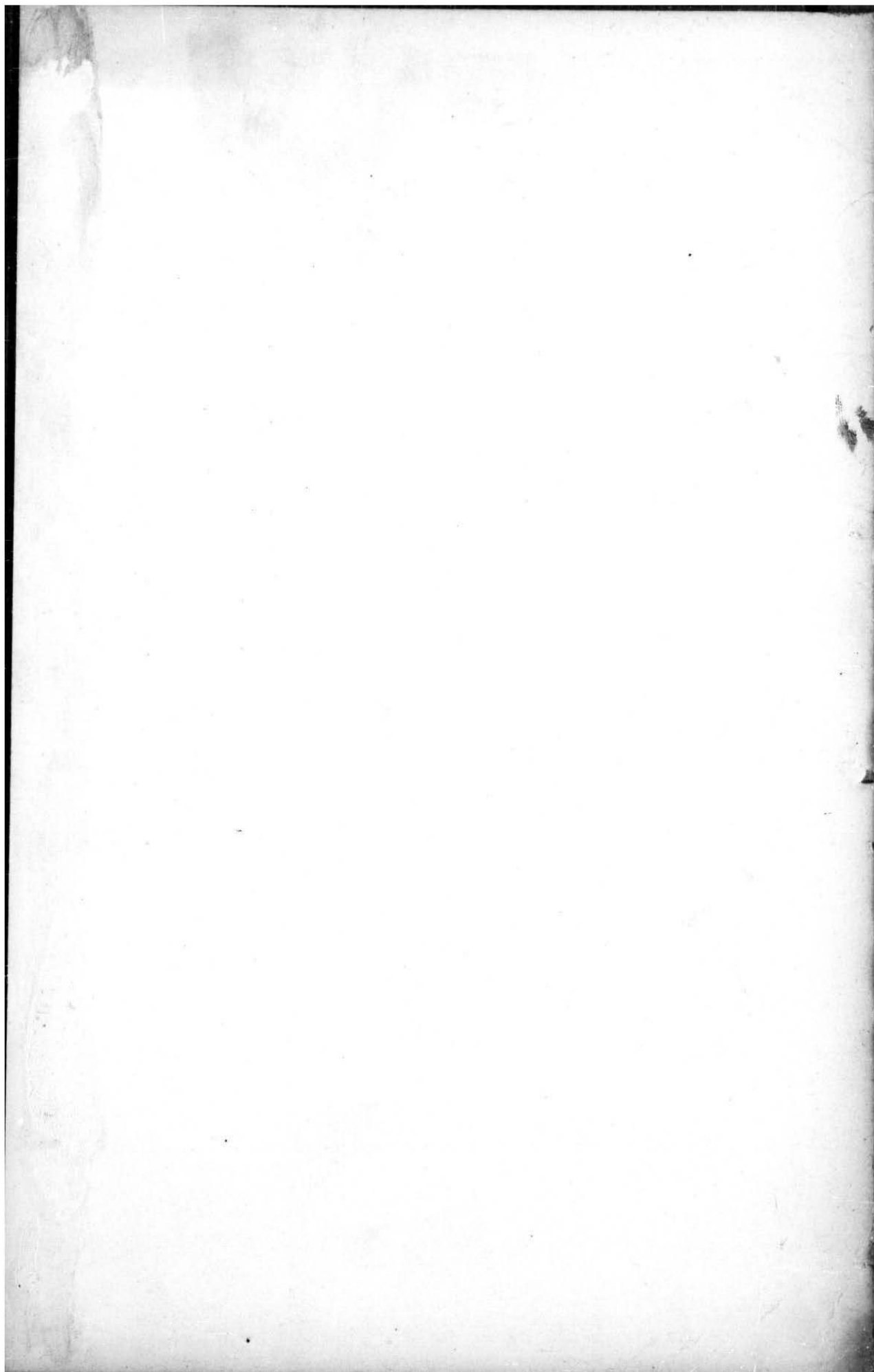
"B" III
Distance Sketch from the Conical
Hill (marked 400') in Distance Sketch A
78W

"B" 4

- (1) SHAN-HAI-KUAN in winter.
 - (i) View from Shore looking seawards.
 - (ii) View looking East showing site of No:1 Fort now demolished and lighthouse occupying its place.
 - (iii) The mouth of the HAI HO (formerly called PEI-HO) at TA-KU. Remains of South Fort after demolition.
- (2)
 - (i) Photograph of boundary stone of CHIH-LI and FENG-TIEN Provinces erected on the South side of the main road from LIN-YU HSIEN (i.e. SHAN-HAI-KUAN) to FENG-TIEN FU (i.e. MUKDEN)
 - (ii) Photograph of boundary stone similar to (i) erected by the side of the railway where the boundary crosses the line.
 - (iii) Blue print copy of sketch of stone referred to in (i) above, with translation of inscription on stone.
- (3) Types of Bridges.
 - (i) 40' span - Near KAO-CH'IAO HO.
 - (ii) 30' span - First bridge East of NING-YUAN.
 - (iii) Temporary pile bridge over TA-LING HO built by Russians.
 - (iv) Permanent bridge in course of construction over TA-LING HO.
- (4) View looking North West from high ground overlooking the village of CH'EN-CHIA-T'UN. Typical of country in CHIN-CHOU Prefecture.
- (5) Typical of country South of railway between TA-LING HO and XYING-K'OU.
- (6)
 - (i) View looking South from SHENG-CHIA-T'AI on the SHENG-KOU HO.
 - (ii) View looking North West from small pass 3 miles South of CH'EN-CHIA-T'UN, showing where the two rivers SHENG-KOU HO and HSIAC-LING HO unite.
- (7)
 - (i) View looking North West from a point South East of PA-TAI-YING-TZU.
 - (ii) View looking South East from a point North West of PA-TAI-YING-TZU.
- (8)
 - (i) View looking North from point 2 miles East of CH'AO-YANG HSIEN showing hill 700 feet above the level of the valley - in the bend of the TA-LING HO on the right bank.
 - (ii) View from point C on sketch plan of CHIN-CHOU Railway Station looking East. Typical of country between NING-YUAN and TA-LING HO.

SKETCHES.

- (i) Distance Sketch of railway crossing over TA-LING HO from East or left bank.
- (ii) Distance Sketch of railway crossing over TA-LING HO from West or right Bank.
- (iii) Views from points A, B, and C, on Plan 17.



PLAN OF RAILWAY PROPERTY
AT
SHAN-HAI-KUAN

SCALE 1 inch to 400 Feet

Feet 100 200 300 400 500 600 700 800 900 1000 1100 1200 1300 1400 1500

A = Locomotives under repair
B = R.S.G. Quarters
From B.R.A. plans